

---

## ADMINISTRATIVE SITE DEVELOPMENT PLAN REPORT & DECISION

---

**REPORT DATE:** September 23, 2014

---

**Project Name:** Hotel at Soutport

---

**Owner:** Michael Christ; 1083 Lake Washington Blvd, Suite 50; Renton, WA 98056

---

**Applicant:** Greg Krape; Hotel at Southport, LLC; 1083 Lake Washington Blvd, Suite 50; Renton, WA 98056

---

**Contact:** Bill Stalzer; Stalzer & Associates; 603 Stewart St, Suite 512; Seattle, WA 98101

---

**File Number:** LUA14-000645, SA-A, SM, MOD, MOD

---

**Project Manager:** Rocale Timmons, Senior Planner

---

**Project Summary:** The applicant is requesting a Minor Master Site Plan Modification, Site Plan Review, Shoreline Substantial Development Permit, Parking Modification, and Upper Story Setback Modification for the construction of a 325,500 square foot, 350 guest room hotel with associated meeting rooms, exercise facility, spa, restaurant, and support facilities. The property is located within the Urban Center North (UCN) land use designation and the Urban Center North - 2 (UCN-2) zoning classification. However, the proposal is vested to: the COR-3 zoning classification as part of the Southport Development Planned Action SFEIS (September 17, 1999); the Southport Level II Master Plan and Shoreline Development Permit (LUA99-189, SA, SM); and subsequent Master Site Plan Modification (dated February 4, 2008). The approximate 11 acre site is currently vacant and contains a gravel parking lot. The hotel would be setback 50 feet from the Lake Washington ordinary high water mark (OHWM) and would have an approximate height of 75 feet within 100 feet of the OHWM and is 125 feet in height as it recedes from the shoreline. The applicant is proposing an interim surface parking lot containing 487 parking spaces on the Southport Office Building site (LUA00-156) until such time the approved offices are constructed. Permanent parking would be provided within shared use parking garages within the office buildings once constructed. The applicant is requesting a modification from RMC 4-4-080 in order to exceed the maximum number of parking stalls allowed by code and reduce the parking stall length required in code. Access to the site is proposed from Lake Washington Blvd via an internal road network to the overall Southport Development site. The site is located within the Shoreline Jurisdiction.

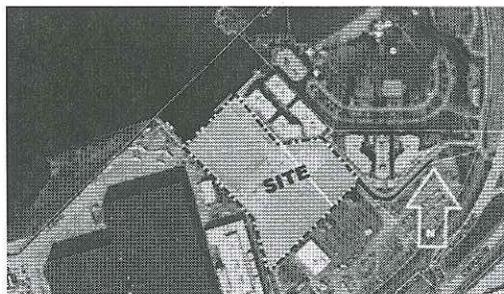
---

**Project Location:** 1083 Lake Washington Blvd

---

**Site Area:** 11.12 acres

---



---

Project Location Map

**A. EXHIBITS:**

- Exhibit 1: Southport Report and Decision
- Exhibit 2: 2014 Master Site Plan Modification Plan
- Exhibit 3: Hotel Site Plan
- Exhibit 4: Landscape Plan
- Exhibit 5: Elevations
- Exhibit 6: Transportation Assessment prepared by TenW (dated May 13, 2014)
- Exhibit 7: Environmental Review Committee Memo (dated July 14, 2014)
- Exhibit 8: Drainage Report prepared by Coughlin Porter Lundeen (dated April 25, 2013)
- Exhibit 9: Geotechnical Report prepared by HartCrowser (dated April 18, 2014)
- Exhibit 10: Aerial Photograph
- Exhibit 11: Vested Commercial Office Residential – 3 Development Standards
- Exhibit 12: Vested Shoreline Master Program Regulations - 1999
- Exhibit 13: Southport Planned Action Ordinance (#4804)
- Exhibit 14: Approved Southport Master Site Plan (LUA99-189/dated July 18, 2000)
- Exhibit 15: Approved Southport Master Site Plan Minor Modification (dated February 4, 2008)
- Exhibit 16: Planned Action Designation (dated April 4, 2000)
- Exhibit 17: Southport FSEIS and Mitigation Document (dated September 17, 1999)
- Exhibit 18: Parking Analysis prepared by TENW (dated April 28, 2014)
- Exhibit 19: 2014 Photo View Analysis
- Exhibit 20: Lighting Plan
- Exhibit 21: Building Mass Study/Rendering
- Exhibit 22: Approved Shoreline Substantial Development Permit (April 7, 2000)
- Exhibit 23: Solar Study
- Exhibit 24: Lake Washington Existing Trail Condition

**B. GENERAL INFORMATION:**

1. **Owner(s) of Record:** Michael Christ  
 1083 Lake Washington Blvd, Suite 50  
 Renton, WA 98056
2. **Comprehensive Plan Land Use Designation:** Urban Center North (UCN) - 1999
3. **Zoning Classification:** Urban Center North-2 (UCN-2)  
*Vested to 1999 Commercial Office Residential-3 (COR-3) (Exhibit 11)*
4. **Existing Site Use:** Vacant/Gravel Parking Lot
5. **Neighborhood Characteristics:**
- a. **North:** Lake Washington
  - b. **East:** Multi-Family Residential (UCN-2 zone)
  - c. **South:** Puget Sound Energy (UCN-2 zone)
  - d. **West:** Boeing Renton Plant (UCN-2 zone)
6. **Site Area:** 484,281 square feet

**C. HISTORICAL/BACKGROUND:**

<u>Action</u>	<u>Land Use File No.</u>	<u>Ordinance No.</u>	<u>Date</u>
Comprehensive Plan	N/A	5099	11/01/2004
Zoning	N/A	5100	11/01/2004
Annexation	N/A	1791	09/09/1959
Planned Action EIS	N/A	4804	10/25/1999
Master Site Plan & Shoreline Substantial Development Permit	LUA99-189	N/A	04/07/2000
Minor Modification for the Southport Master Site Plan	LUA99-189	N/A	02/04/2008
Administrative Site Plan Review (Bristol Apartments)	LUA01-144	N/A	02/22/2002
Administrative Site Plan Review (Southport Office Bldgs)	LUA00-156	N/A	02/05/2001

**D. PUBLIC SERVICES:**

1. **Existing Utilities**
- a. **Water:** Water service would be provided by the City of Renton. There is an existing 12-inch water main located within the internal access road for the adjacent properties east of the proposed

hotel, known as the Bristol Apartments, (refer to City project plans no. W-2882 and W-3291).

The static water pressure is approximately 128 psi at ground elevation of 22 feet.

- b. Sewer: Sewer service would be provided by the City of Renton. There is an 8-inch sewer main in an easement on the site.
  - c. Surface/Storm Water: There is a private storm drainage conveyance system and water quality treatment on site.
2. **Streets**: There are partial street improvements in Lake Washington Blvd NE to accommodate the proposed use.
  3. **Fire Protection**: City of Renton Fire Department

**E. APPLICABLE SECTIONS OF THE RENTON MUNICIPAL CODE:**

1. **Chapter 2 Land Use Districts**
  - a. Section 4-2-020: Purpose and Intent of Zoning Districts
  - b. Section 4-2-070: Zoning Use Table
  - c. Section 4-2-120: Development Standards for Commercial Zoning Classifications (1999) (Exhibit 11)
2. **Chapter 3 Land Use Districts**
  - a. Section 4-3-090: Shoreline Regulations (1999) (Exhibit 12)
3. **Chapter 4 Property Development Standards**
4. **Chapter 6 Streets and Utility Standards**
  - a. Section 4-6-060: Street Standards
5. **Chapter 9 Procedures and Review Criteria**
  - a. Section 4-9-200: Site Plan Review
6. **Chapter 11 Definitions**

**F. ADMINISTRATIVE SITE PLAN REVIEW FINDINGS OF FACT:**

1. The Southport Hotel Site Plan implements Phase III of the Southport Master Site Plan. The hotel project is located on Lots 1 and 4 of the Southport development and includes 350 guest rooms, meeting rooms, an exercise facility, indoor pool and spa, restaurant, lobbies, administrative offices, support facilities, and a waterfront promenade with a pedestrian walkway connection to Gene Coulon Park.
2. The property is located within the Urban Center North (UCN) land use designation and the Urban Center North - 2 (UCN-2) zoning classification. However, the proposal is vested to the COR-3 zoning classification as part of the Southport Development Planned Action Final Supplemental Environmental Impact Statement (SFEIS) (September 17, 1999); the Southport Level II (Master Site Plan) and Shoreline Development Permit (LUA99-189, SA-A, SM/Exhibits 14 and 22); and subsequent Master Site Plan Modification (dated February 4, 2008/Exhibit 15).
3. The FSEIS and mitigation document was issued for the Southport Planned Action in September 1999 (Exhibit 17). Ordinance #4804 was adopted by the City Council in October 1999 designating a Planned Action of the subject site (Exhibit 13).
4. The objective of the Southport Planned Action SFEIS was to complete the environmental review in advance of the overall conceptual Master Site Plan. Project applications and development phases are then allowed to proceed without additional environmental review provided the proposed development is consistent with the plan alternatives and level of development that was analyzed in the EIS process.

5. On April 4, 2000, the City determined that the Southport Master Site Plan was consistent with both the range of plan alternatives that were evaluated in the FSEIS and with the development levels or parameters as specified in the Planned Action Ordinance (Exhibit 16). Any subsequent applications to develop the property consistent with the Master Site Plan would also be covered by the Planned Action and would not require additional SEPA review.
6. Since the 2000 Master Site Plan approval, the City has issued Site Plan approvals for two phases (of the three) of the Southport Development: Phase I, The Bristol, a mixed/use residential complex and Phase II, the three building office complex which is yet to be constructed.
7. The Southport Master Site Plan remains valid and effective through July 18, 2015.
8. The City approved a minor modification to the Master Site Plan on February 4, 2008 (Exhibit 15). As modified the Master Plan permitted two 50 foot tall multifamily residential buildings with a total of 383 units (currently constructed); three 125 foot tall office buildings with a total of 750,000 square feet of office space; and a 75 and 125 foot tall hotel with 355 rooms (212,502 square feet); 40,000 square feet of retail space, 30,000 square feet for general use retail and 10,000 square feet for restaurants; a promenade along the Lake Washington waterfront including a trail connection to Gene Coulon Park; and off-site transportation improvements.
9. A Shoreline Substantial Development Permit for the Southport Development Level II (Master) Site Plan was approved on April 7, 2000 (LUA99-189, SA-A, SM/Exhibit 14). The requested Shoreline Substantial Development Permit is not intended to limit the use of the approved Shoreline Substantial Permit for the approved Southport Office Buildings. The requested permit is necessitated by changes to the height of the hotel and the review is limited in scope solely to height.
10. The proposed hotel is set back 50 feet from the shoreline and consists of two building elements: a lower section capped by a large terrace and a ballroom with large windows for views of Lake Washington and distant vistas and a C-shaped section that increases in height from 75 feet to 125 feet as it steps back from the 50-foot setback line. The break in height occurs at the 100 foot setback line as required by the COR-3 zoning. The taller section occupies approximately 50% of the width of the site. The terrace on the lower section of the hotel is located approximately 28 feet above the waterfront promenade is a wide, deep terrace enclosed by the two wings of the "C". The 75 foot high portions of the building are capped by two large terraces connected by a walkway. The 125-foot high portion of the building is oriented to the internal street shared with the 125-foot high office buildings in approved Phase II.
11. The existing waterfront consists of a concrete bulkhead across the entire site frontage along Lake Washington. The depth of the water at the bulkhead varies from approximately 6 feet at the eastern end to approximately 14 feet at the western end and the top of the bulkhead is 2 to 3 feet above the ordinary high water mark.
12. No work is proposed below the ordinary high water mark of Lake Washington.
13. The building entrance is proposed along the southern facade of the building with an orientation to the interior surface parking area. The main entrance to the hotel is located at this level along the building's south facade (Exhibit 5).
14. The building materials vary and are a combination of cement panels, cast in place concrete, corrugated metal paneling, glass/aluminum, and brick (Exhibit 5).
15. A pedestrian bridge is proposed connecting the two guestroom wings and their terraces.
16. Access to the site would be provided via an existing internal private street extended from Lake Washington Boulevard NE.

17. An interim surface parking lot containing 487 parking spaces would be located on the site of the approved Southport Office Building Site Plan (LUA00-156).
18. RMC 4-4-080 requires the applicant limit the number of parking spaces based on the number of hotel guest rooms and number of employees. The applicant is requesting a parking modification in order to increase the number of stalls located on site from the 467 parking space allowance to 487 parking spaces for the restaurant.
19. RMC 4-4-080 requires the applicant provide a parking stall length of 20 feet. The applicant is requesting a parking modification to decrease the length of parking stalls from 20 feet to 19 feet.
20. There is an existing public promenade which connects Gene Coulon park (to the east) to the Boeing site (to the west) (Exhibit 3).
21. The applicant anticipates commencing construction in Fall of 2014.
22. The Planning Division of the City of Renton accepted the above master application for review on May 16, 2014 and determined it complete on May 28, 2014. The project complies with the 120-day review period.
23. No agency comments were received during the 14-day comment period. A comment letter was received by The Boeing Company on September 16, 2014.
24. Representatives from various city departments have reviewed the application materials to identify and address issues raised by the proposed development. These comments are contained in the official file, and the essence of the comments have been incorporated into the appropriate sections of this report and the Departmental Recommendation at the end of this report.
25. The proposal requires a minor modification to the approved Master Site Plan (Level 1 Site Plan) as modified on February 4, 2008 (Exhibit 15). The following table contains project elements intended to comply with the minor modification to approved Site Plans criteria, as outlined in RMC 4-9-200H.2:

**Table A: MINOR MODIFICATION TO AN APPROVED MASTER PLAN OR SITE PLAN**

*Minor modifications may be permitted by administrative determination. To be considered a minor modification, the amendment must not:*

✓	<b>Involve more than a ten percent (10%) increase in area or scale of the development in the approved plan.</b>			
	<i>Staff Comment: The requested modification to the approved Southport Master Plan (Exhibit 14) as modified on February 4, 2008 (Exhibit 14) includes a reduction in the number of hotel rooms from 355 to 350 rooms and a corresponding reduction in area designated to guest rooms and lobbies (Exhibit 2). It should be noted, the proposed Minor Modification represents a 1.5% increase in the scale/square footage of development from the 2000 Master Site Plan approval:</i>			
	Use	2000 Southport Master Site Plan (Exhibit 13)	2008 Southport Master Site Plan Minor Modification (Exhibit 14)	2014 Southport Master Site Plan Minor Modification
	Multi-Family Residential Units	394	383	383
	Multi-Family Residential SF	383,488	321,776	321,776
Retail (Specialty) SF	30,000	30,000	30,000	
Restaurant SF	10,000	10,000	10,000	

	<i>Hotel Rooms</i>	220	355	350
	<i>Hotel SF</i>	105,600	212,502	156,195
	<i>Commercial SF</i>	720,205	750,000	750,000
	<b><i>Total Square Feet</i></b>	<b>1,249,293</b>	<b>1,324,278</b>	<b>1,267,971</b>
✓	<p><b>Have a significantly greater impact on the environment and/or public facilities than the approved plan.</b></p> <p><i>Staff Comment:</i> The development levels proposed under the requested Southport Master Site Plan Minor Modification would not follow development parameters of any single alternative action plan analyzed by the EIS documents (Exhibit 17). However, the development levels are consistent with the maximum development parameters included in the full range of the three alternative action plans. The applicant submitted a trip generation analysis (Exhibit 6) for the requested reduction in hotel rooms/space and provides a comparison to the PM peak hour trip generation analyses documented in the Southport Master Site Plan approval (Exhibit 14) and the 2008 Southport Master Site Plan Minor Modification (Exhibit 15). The transportation assessment notes that that the PM Peak Hour trips generated by the uses in the proposed Minor Modification to the Southport Master Site Plan (1,314 trips) are less than the number of PM Peak Hour trips in the approved 2008 Minor Modification to the Master Site Plan due to a reduction in the number of hotel rooms. Additionally, the proposed number of trips is within the range of PM Peak Hour trips analyzed in the Planned Action (1,355 trips). As noted by TENW, the proposed Minor Modification to the Southport Master site Plan does require any mitigation measures beyond those required for the Level II Site Plan approved in 2000.</p>			
✓	<p><b>The modification does not change the boundaries of the originally approved plan.</b></p> <p><i>Staff Comment:</i> The boundaries in the approved 2000 Southport Master Site Plan (Exhibit 14) remain unchanged in the requested minor modification.</p>			

26. The proposal requires Site Plan Review. The following table contains project elements intended to comply with Site Plan Review decision criteria, as outlined in RMC 4-9-200.E:

<b>Table B: SITE PLAN REVIEW</b>	
<b>a. COMPREHENSIVE PLAN COMPLIANCE AND CONSISTENCY</b>	
The site is vested (1999) to the Center Office Residential Comprehensive Plan land use designation.	
<i>Staff Comment:</i> The City concluded that the approved Southport Master Site Plan-2000 is in conformance with the elements and policies of the Comprehensive Plan (Exhibit 14). The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').	
<b>b. Zoning Compliance and Consistency:</b>	
The proposal is vested to the Center Office/Residential 3 (COR-3) on the City's 1999 Zoning Map; RMC 4-2-120B (Exhibit 11). The proposal is compliant with the following COR-3 development standards if all conditions of approval are met:	
N/A	<p><b>Density:</b> The maximum density of the COR-3 zone is 50 du/acre.</p> <p><i>Staff Comment:</i> Not applicable.</p>
N/A	<p><b>Lot Dimensions:</b> Per RMC 4-2-120B there are no minimum lot dimensions.</p>

	<p><u>Staff Comment:</u> Not applicable.</p>
✓	<p><b>Lot Coverage:</b> Per RMC 4-2-120B the allowed lot coverage is 65 percent or 75% if parking is provided within the building or within an on-site parking garage.</p> <p><u>Staff Comment:</u> The City has concluded the uses within the approved Southport Master Site Plan, specifically the hotel, to be in conformance with the lot coverage allowed in the vested COR-3 zoning classification (Exhibit 14). The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</p> <p>All buildings, including the proposed hotel, would have a building footprint of 367,084 square feet on the 620,640 square foot (upland) site resulting in a building lot coverage of approximately 59.14 percent.</p>
Compliant if all conditions of approval are met	<p><b>Setbacks:</b> Per RMC 4-2-120B the COR-3 zone has not setbacks from property lines.</p> <p><u>Staff Comment:</u> As part of the Southport FSEIS and Mitigation Document (Exhibit 17) a minimum building setback of 10-30 feet is required to be provided along the western boundary with an average setback of 20 feet. It appears the proposal complies with the minimum western property line setback. However, it is unclear if the proposal meets the average setback requirement of 20 feet. Therefore staff recommends as a condition of approval the applicant be required to demonstrate compliance with the required average western property line setback of 20 feet prior to building permit approval.</p>
✓	<p><b>Shoreline Setback:</b> Per RMC 4-2-120B the COR-3 zone has a special shoreline setback of 50 feet.</p> <p><u>Staff Comment:</u> The City has concluded the shoreline setback within the approved Southport Master Site Plan, specifically for the hotel, to be in conformance with the shoreline setback allowed in the former COR-3 zoning classification (Exhibit 14). The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</p> <p>The proposed shoreline setback is 50 feet.</p>
✓	<p><b>Building Height:</b> Per RMC 4-2-120B building height is 125 feet and restricted to 75 feet within 100 feet of the OHWM.</p> <p><u>Staff Comment:</u> The City has concluded the heights within the approved 2008 Minor Modification to the Southport Master Site Plan, specifically for the hotel, to be in conformance with the height allowances in the former COR-3 zoning classification (Exhibit 15). The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</p> <p>The hotel consists of two 75-foot high guest room wings enclosing a wide, deep central terrace located approximately 28 feet above the Lake Washington promenade. As the building recedes from the promenade, 100 feet from the Lake Washington OHWM, the guest room wings step up in height to 125 feet above the datum point. The proposal complies with the height requirements of the zone.</p> <p>The combined maximum building height and site elevation would not exceed the maximum</p>

<p><b>Compliant if all conditions of approval are met</b></p>	<p><i>height of 179 feet above sea level, meeting current FAA maximum height for structures.</i></p> <p><b>Screening:</b> Outdoor storage is not permitted in the COR-3 zone. All mechanical equipment and outdoor service and storage areas shall be screened to reduce visibility, noise, and related impacts while allowing accessibility for providers and users. All roof-top equipment must be screened by public view.</p> <p><i>Staff Comment:</i> Refuse and garbage containers would be located in the service area along the western property boundary and screened from public view. Hotel rooftop mechanical equipment would be screened by a screen wall covered by metal siding (Exhibit 5). It is unclear if all surface-mounted utility and mechanical equipment have been screened from public view. Therefore, staff recommends as a condition of approval, the applicant submit screening detail for all surface mounted utility and mechanical equipment to the satisfaction of the Current Planning Project Manager prior to building permit approval.</p>
	<p><b>Parking:</b> The parking regulations, RMC 4-4-080, require a specific number of off-street parking stalls be provided for the proposed use.</p> <p>The code requires 1 space for each guest room or dwelling unit plus 2 for each 3 employees.</p> <p><i>Staff Comment:</i> The proposed hotel contains 350 guest rooms, resulting in a code requirement for 350 parking spaces. The hotel would have 175 employees resulting in a code requirement for 117 additional spaces for hotel employees. The applicant is requesting a parking modification to increase the number of stalls located on site from the 467 parking space allowance to 487 parking spaces for the hotel. Additionally, RMC 4-4-080 requires the applicant provide a parking stall length of 20 feet. The applicant is also requesting a parking modification in order to decrease the length of a parking stall from 20 feet to 19 feet.</p> <p>Section 4-9-250 allows a grant of relief from the requirements of the code which permits construction in a manner that otherwise is prohibited, provided the modification meets the following criteria (pursuant to RMC 4-9-250.B.5):</p> <ol style="list-style-type: none"> <li>a. Will meet the objectives and safety, function, appearance, environmental protection and maintainability intended by the Code requirements, based upon sound engineering judgment; and</li> <li>b. Will not be injurious to other property(ies) in the vicinity; and</li> <li>c. Conform to the intent and purpose of the Code; and</li> <li>d. Can be shown to be justified and required for the use and situation intended; and</li> <li>e. Will not create adverse impacts to other property(ies) in the vicinity.</li> </ol> <p>The applicant submitted a parking analysis as part of the submittal for Site Plan Review prepared by TENW, dated April 28, 2014 (Exhibit 18). According to the parking analysis the estimated peak parking demand for guests and employees for a 350 room hotel is 315 parking spaces. The proposed 487 stall parking lot would serve as an interim lot until such time as future office buildings to be located on Lot 4 are constructed. Ultimately the parking needed to serve the proposed hotel would be located in structured parking facilities shared by the hotel and the office complex.</p> <p>The applicant contends the additional 20 parking spaces beyond code would create an opportunity to provide public parking for people using the waterfront promenade and trail connection to Gene Coulon Park; visiting hotel guests; and/or non-hotel guests using the hotel facilities.</p> <p>With respect to the length of parking the stalls the current City parking standards, for the</p>

	<p>subject site, require the length of a standard parking stall be 19 feet. The applicant contends that closer conformity to Current City standards meets the purposes intended by the Code and is based on sound engineering judgment.</p> <p>Staff concurs with the applicant's justification for the reduction of parking stall length in order to better comply with current City code. The requested reduction in stall length would not be injurious to other properties in the vicinity.</p> <p>However, staff is not supportive of the request for an increase in the number of parking stalls from 467 to 487 surface parking stalls. While Southport is an urban development with intense existing and planned uses, structured parking was originally envisioned as part of the hotel development. The proposed large parking area would have aesthetic impacts on surrounding properties despite its being temporary. The use of interior parking lot landscaping would improve the aesthetic quality and soften the appearance of the proposed surface parking area.</p> <p>Compliance with the allowed number of parking stalls provides additional opportunities to incorporate interior parking lot landscaping in the large parking area until such time structured parking stalls are constructed. Therefore, the proposed parking modification to increase the number of allowed parking stalls is recommended for denial. Staff recommends the applicant be required to revise the site to eliminate at least 20 parking stalls and include additional intervening landscaping <u>within</u> the surface parking area to the satisfaction of the Current Planning Project Manager. The revised site and landscaping plan shall be submitted to and approved by the Current Planning Project Manager prior to construction permit approval.</p>
<p><b>Unknown</b></p>	<p><b>Signs:</b></p> <p><u>Staff Comment:</u> The applicant did not submit a signage package for the proposed hotel and therefore could not be reviewed at this time. The applicant would be required to comply with the signage requirements outlined in RMC 4-4-100 at the time of sign application.</p>
<p>✓</p>	<p><b>Critical Areas:</b></p> <p><u>Staff Comment:</u> The proposal is subject to the Shoreline Master Program (Exhibit 12) due to the site's proximity to Lake Washington. The site is located within the Urban Environment. A Shoreline Substantial Development Permit was issued for the Southport Master Site Plan in April of 2000 (Exhibit 22). However, the proposed hotel requires additional review under the Shoreline Master Program in order to account for changes included in the approved 2008 Minor Modification to the Master Site Plan (see Exhibit 15) and the proposed Minor Modification to the Master Site Plan (see Table A). The proposal is compliant with the vested Shoreline Master Program (see detailed discussion in Table C: Vested Shoreline Master Program Regulations).</p>
<p>✓</p>	<p><b>Modulation and Articulation:</b> Incorporate building modulation to reduce the overall bulk and mass of buildings.</p> <p><u>Staff Comment:</u> The proposed hotel building is adjacent to the Bristol Apartments, the Boeing Facility, and future office buildings. In order to create articulation and modulation and reduce the overall bulk and mass of the hotel building the applicant has proposed changes to the exterior materials on the building, vertical plane changes, changes in the color, superior window design, and contrast between solid wall panels and transparent glass areas.</p> <p>The applicant is proposing a variety of concrete wall panel exterior materials, including cast in place concrete and architectural concrete wall panels which vary in color and texture on Floors 1, 2 and 3. Floors 4 through 7 would have a combination of brick and cement panels</p>

	<p><i>similar in character to the neighboring Bristol Apartments. Proposed Floors 8 through 12 would also have cement panels similar in color to those used on the Bristol Apartments and proposed for use for the future office buildings (Exhibit 5).</i></p> <p><i>The applicant has designed exterior wall panels to create a plinth/platform for each change of material above. The concrete base would extend 6-8 inches from the face of the brick wall above and the brick would extend 4-6-inches from the face of the cement wall panels above.</i></p> <p><i>The applicant is proposing storefront windows on Floors 1-3 which serves to maximize views to the water and surroundings. All windows on the upper stories are punched window style. The upper level window frames are flush with the cement wall panel exterior, recessed 4-inches from the exterior face of the proposed brick, and 6-8 inches within the concrete podium.</i></p> <p><i>The applicant has yet to select final colors for the proposed hotel but indicates the colors used would be similar to those used on the Bristol Apartments. Staff recommends, as a condition approval, the applicant provide a material/colors board to the Current Planning Project Manager prior to the building permit approval. The materials board shall demonstrate compatibility with the Bristol Apartments and future office buildings.</i></p> <p><i>The articulation and modulation of the hotel building facades is in keeping with the surrounding buildings in the neighborhood.</i></p>
✓	<p><b>Upper Story Setbacks:</b> Per RMC4-2-120 Buildings or portions of buildings which exceed 50-feet in height which are located within 100 feet of the shoreline shall include upper story setbacks for the façade facing the shoreline and for facades facing publicly accessible plazas as follows: The minimum setback for a fifth story and succeeding stories shall be 10 feet. Projects not meeting the upper story setbacks may be approved through a modification process. Application may be made for modification of the upper story setback standards when superior design is demonstrated pursuant to RMC 4-9-250D. For a modification to be granted, the project must also comply with the decisions and design criteria stipulated in RMC 4-9-250D2 and D4.</p> <p><u>Staff Comment:</u> <i>The applicant contends that the proposed hotel offers a superior design solution as an alternative to the upper story setback requirement. Pursuant to RMC 4-9-250D the applicant has requested a modification to the upper story setbacks requirement found in RMC 4-2-120B.</i></p> <p><i>Section 4-9-250 allows a grant of relief from the requirements of the code which permits construction in a manner that otherwise is prohibited, provided the modification meets the following criteria:</i></p> <ul style="list-style-type: none"><li><i>a. Will meet the objectives and safety, function, appearance, environmental protection and maintainability intended by the Code requirements, based upon sound engineering judgment; and</i></li><li><i>b. Will not be injurious to other property(ies) in the vicinity; and</i></li><li><i>c. Conform to the intent and purpose of the Code; and</i></li><li><i>d. Can be shown to be justified and required for the use and situation intended; and</i></li><li><i>e. Will not create adverse impacts to other property(ies) in the vicinity; and</i></li><li><i>f. In comparison to the standard upper story setbacks, the proposed building design will achieve the same or better results in terms of solar access to the public shoreline trails/open space and publicly accessible plazas; the building will allow access to sunlight along the public trail/open space system and plazas abutting</i></li></ul>

	<p><i>the shoreline during daytime and seasonal periods project for peak utilization by pedestrian; and</i></p> <p><i>g. The building will create a step in perceived height, bulk and scale in comparison to buildings surrounding the subject property.</i></p> <p><i>As an alternative to providing multiple smaller steps beyond 75 feet in height the applicant is proposing a single step for the hotel structure. The applicant contends the single step better relates to the existing urban character of the site, in that upper story setbacks were not used on any other building in the vicinity. To the west, the Boeing Assembly Building is approximately 125 feet in height primarily a vertical wall without relief. To the north, the existing waterfront promenade is used by pedestrians. To the east, the Bristol Apartments are approximately 75 feet in height, with window penetrations and balconies, but without stepping of the facade. To the south, the approved future office buildings would be built of flat metal wall panels and glass, rising to approximately 125 feet without upper story setbacks.</i></p> <p><i>To ensure compatibility with its surroundings, the applicant contends the building design responds to the Lake Washington waterfront and nearby structures. A majority of the north façade along Lake Washington, and the pedestrian promenade, is approximately 28 feet in height. There are two major terraces; one over the grand ballroom, and the other located in between the two guestroom wings. On the east façade, the guestroom wing of the hotel is similar in scale to the Bristol Apartments by stepping from 75 feet up to 125 feet from north to south. The 125 feet height along the south façade of the hotel would be similar to the scale of the Boeing facility and future office buildings (Exhibit 5).</i></p> <p><i>The taller portion of the hotel occupies approximately half of the width of the site along the Lake Washington frontage. The remaining width is the much lower terrace at approximately 28 feet in height. The applicant submitted a solar study (Exhibit 23) and a massing diagram (Exhibit 21) demonstrating a reduction in shade and shadows effects on the pedestrian walkway at noon during the spring and fall equinox. Additionally, the low west terrace and low terrace in the middle of the building allow more sunlight to reach the promenade during the summer solstice than does the code-required upper story setbacks design solution.</i></p> <p><i>Staff concurs that the proposed single step of the building is the most appropriate solution for a high-rise hotel. The proposed design solution creates less shade/shadow effects on the promenade and pedestrian walkway and achieves greater compatibility with its surroundings than does the use of upper story setbacks thereby conforming to the intent and purpose of the code. The hotel design would not create adverse impacts to other properties in the vicinity and would likely offer benefits in the form two large water-oriented terraces at the 8th floor, offering inviting outdoor function spaces with unobstructed water views of the promenade, Lake Washington, and distant vistas. The single upper story setback would not be injurious or create adverse impacts to the other properties in the vicinity.</i></p>
<p><b>c. DESIGN REGULATION COMPLIANCE AND CONSISTENCY:</b></p> <p><i>Not applicable.</i></p>	
<p><b>d. PLANNED ACTION ORDINANCE AND DEVELOPMENT AGREEMENT COMPLIANCE AND CONSISTENCY:</b></p>	
<p>✓</p>	<p><i>Staff Comment: The City's Environmental Review Committee determined that the proposal meets the criteria outlined in the Planned Action Ordinance (Ordinance #4804) and qualifies as a planned action, the proposal shall not require a SEPA threshold determination, preparation of an EIS, or be subject to further review pursuant to SEPA (Exhibit 7).</i></p>
<p><b>e. OFF SITE IMPACTS:</b></p>	

✓	<p><b>Structures:</b> Restricting overscale structures and overconcentration of development on a particular portion of the site.</p> <p><i>Staff Comment:</i> The City has concluded the heights within the approved 2008 Minor Modification to the Southport Master Site Plan, specifically for the hotel, to be in conformance with the height allowances in the former COR-3 zoning classification (Exhibit 15). The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</p> <p>In addition to the minimization of the building mass to approximately 50% of the site width, the hotel would contain substantial modulation of major building elements and a 100 foot setback of the taller upper portion of the hotel from the ordinary high water mark.</p> <p>See additional discussion under Structure Scale.</p>
✓	<p><b>Circulation:</b> Providing desirable transitions and linkages between uses, streets, walkways and adjacent properties.</p> <p><i>Staff Comment:</i> Physical transition and linkage elements would be provided along the north, east and south property lines. Along the north frontage, the pedestrian walkway easement on Lots 1 and 4 extends the pedestrian route to the east property line providing a pedestrian and bicycling connection to Gene Coulon Park for the entire length of the Southport shoreline. The sidewalk on the east side of the hotel provides a direct pedestrian route to the waterfront area.</p> <p>Along the south side of the hotel, the walkway and hard surface treatment of the hotel main entrance turnaround would provide a connection to the approved office buildings to the south as well as a direct link to the internal street which connects to Lake Washington Blvd NE. Similar vehicular links would be provided at driveways located along the east side of the hotel and at the southeast corner of the surface parking lot. Landscaped areas are proposed along the east, south and west sides of the parking lot in order to provide a visual transition between the parking lot and surrounding uses.</p> <p>The SFEIS included mitigation measures requiring water access and trail connections from Gene Coulon Park to the Cedar River Trail by a dedicated public recreation easement (Exhibit 17). Three of the four required public recreation easements have been provided (Recording numbers 20051021000894, 20051021000895, and 20051021000896). The fourth recreation easement over the flume/water adjacent to Boeing property has yet to be recorded. The recreation easement should traverse north/south, and connect to existing east/west walkway easement for a future continuous trail connection from Gene Coulon Park to the Cedar River Trail (Exhibit 24). While the proposed hotel phase is not contingent upon the fulfillment of the SFEIS mitigation measure, the remaining public recreation easement will remain a requirement of the SFEIS.</p>
✓	<p><b>Loading and Storage Areas:</b> Locating, designing and screening storage areas, utilities, rooftop equipment, loading areas, and refuse and recyclables to minimize views from surrounding properties.</p> <p><i>Staff Comment:</i> See Screening discussion above.</p>
✓	<p><b>Views:</b> Recognizing the public benefit and desirability of maintaining visual accessibility to attractive natural features.</p> <p><i>Staff Comment:</i> The applicant has submitted revised view impacts studies from various points in the City (Exhibit 19). Views depicting the redeveloped site from Mercer Island and north of</p>

	<p><i>Gene Coulon Park were not provided due to the distance from the site. Other than the residential uses constructed as part of the Southport Master Site Plan the proposal is not adjacent to residential uses or residential zoned property. While it is likely that views from areas of the City that are at higher elevations would be affected there would not be any views from residential areas or public areas obstructed.</i></p> <p><i>The maintenance of visual accessibility to Lake Washington is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</i></p>
	<p><b>Landscaping:</b> Using landscaping to provide transitions between a development and surrounding properties to reduce noise and glare, maintain privacy, and generally enhance the appearance of the project.</p> <p><i>Staff Comment: A conceptual landscape plan was submitted with the project application (Exhibit 4). The landscape plan includes a general planting plan. The use of paving patterns, planting beds and site walls appear to reinforce the architectural elements of the proposed structure. It is unclear, without specific detail, if the proposed landscape planting plan provided a diverse mix of vegetation needed to meet the intent of the code.</i></p> <p><i>Staff recommends, as a condition of approval, the applicant submit a revised and detailed landscape plan complying with the following: a mixture of trees, shrubs, and groundcover. The revised detailed landscape plan, shall be submitted to, and approved by, the Current Planning Project Manager prior to construction permit issuance.</i></p> <p><i>Underground irrigation systems are required to be installed and maintained for all landscaped areas. The irrigation system shall provide full water coverage of the planted areas specified on the plan.</i></p>
✓	<p><b>Lighting:</b> Designing and/or placing exterior lighting and glazing in order to avoid excessive brightness or glare to adjacent properties and streets</p> <p><i>Staff Comment: The applicant provided a lighting plan as part of their submittal package (Exhibit 20). All exterior lighting would have full cutoff fixtures and be located to avoid excessive brightness or glare on adjacent properties and streets. Directional lighting and shading provisions for all light standards along the Lake Washington promenade is required to be implemented.</i></p>
<p><b>f. ON-SITE IMPACTS:</b></p>	
✓	<p><b>Structure Placement:</b> Provisions for privacy and noise reduction by building placement, spacing and orientation.</p> <p><i>Staff Comment: The requested placement of the proposed hotel is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part of the subject application (see Table 'A').</i></p> <p><i>The entrance has a large porte cochere to provide both an architectural entry statement and weather protection for hotel guests, meeting attendees, and visitors arriving and departing by car, van, or tour bus.</i></p> <p><i>Location of the hotel's main vehicular and pedestrian entrance internally on the site is surrounded by buildings, serves pedestrian and vehicle needs, reduces noise impacts on surrounding uses, and retains privacy for hotel users and nearby uses. The building, specifically the terraces and hotel restaurant, would be oriented to views of Lake Washington</i></p>

	<p><i>and distant vistas.</i></p> <p><i>Increased noise levels would be primarily associated with construction and would, therefore be temporary in nature. As part of the SFEIS the applicant is required to conduct specific studies on the potential of significant HVAC noise impacts (Exhibit 17). Therefore staff recommends, as a condition of approval, the applicant be required to submit a noise impact study related to potential HVAC equipment noise prior to building permit approval. If studies indicate that HVAC equipment noise would adversely impact residential development on the site, mitigation can be implemented consisting of noise barriers placed around the HVAC units, selection of alternative equipment with reduced noise levels, or provision of silencers for air intake and exhaust.</i></p>
<p>✓</p>	<p><b>Structure Scale:</b> Consideration of the scale of proposed structures in relation to natural characteristics, views and vistas, site amenities, sunlight, prevailing winds, and pedestrian and vehicle needs.</p> <p><i>The hotel complies with the height standards in the COR-3 zoning. According to the massing study provided (Exhibit 21), the hotel building uses much less of the development envelope than permitted by the COR-3 zone. The scale and bulk of the building is reduced through the use of differing materials on the building facades, building articulation and modulation. The building materials vary and are a combination of cement panels, cast in place concrete, corrugated metal paneling, glass/aluminum, and brick (Exhibit 5).</i></p> <p><i>The proposed structure would not have a significant impact on light access or air movement on adjacent properties. The use of the project is not influenced by factors of light or air. The location of the terraces on the north side of the building would provide protection from southerly prevailing winds and the design of the structure would not result in excessive shading of the property.</i></p> <p><i>The hotel building mass occupies approximately 50% of the width of the site with the remaining portion devoted to a large terrace. To avoid the impression of an oversized structure, the "C" shaped building contains considerable modulation including two "wings" each set back 50 feet from Lake Washington, with the lower 75 foot high portion capped by rooftop terraces, and the taller 125 foot portion set back an additional 50 feet from Lake Washington. The 125 foot high middle portion of the "C" is set back approximately 145 feet from the lake and includes a large terrace approximately 30 feet above the promenade.</i></p> <p><i>The applicant provided solar studies (Exhibit 23) which demonstrates the hotel design maximizes the sun exposure for the terrace and the waterfront promenade area during the peak use times of the year. The use of trees in some of the perimeter landscaping and along the street would provide shading of sidewalks and parking areas with little, impact on adjacent properties. Staff has recommended the applicant provide a detailed landscape plan to be approved by the Current Planning Project Manager prior to construction permit approval.</i></p>
<p>✓</p>	<p><b>Natural Features:</b> Protection of the natural landscape by retaining existing vegetation and soils, using topography to reduce undue cutting and filling, and limiting impervious surfaces.</p> <p><i>Staff Comment: The site consists of a gravel parking lot and little vegetation in the areas not taken up by the lot. None of the existing vegetation on the site is desirable natural landscape. Soil removal is not anticipated as the goal is to balance cuts and fills on the site. Fifteen small caliper white birch trees along the northeast boundary of the development site are being removed to be used at other locations in the Southport development.</i></p> <p><i>Due to the compact, urban nature of the hotel concept, the developed portion of the site will be primarily impervious surfaces. Pervious areas are planned to consist primarily of</i></p>

	<p><i>perimeter landscaped buffers. Following complete build out the Southport Development is anticipated to have impervious surface cover of approximately 85%.</i></p>
✓	<p><b>Landscaping:</b> Use of landscaping to soften the appearance of parking areas, to provide shade and privacy where needed, to define and enhance open spaces, and generally to enhance the appearance of the project. Landscaping also includes the design and protection of planting areas so that they are less susceptible to damage from vehicles or pedestrian movements.</p> <p><u>Staff Comment:</u> See Landscaping discussion above.</p>
<b>g. ACCESS:</b>	
✓	<p><b>Location and Consolidation:</b> Providing access points on side streets or frontage streets rather than directly onto arterial streets and consolidation of ingress and egress points on the site and, when feasible, with adjacent properties.</p> <p><u>Staff Comment:</u> Access is provided via an internal private street connected to Lake Washington Blvd NE. Access to Lake Washington Blvd NE is coordinated with the Puget Sound Energy storage site and Gene Coulon Park but cannot be consolidated due to existing conditions.</p> <p><i>Adequate and safe vehicular access improvements to and from the project site and neighboring properties have been completed or are planned for construction within the next year.</i></p> <p><i>Creation of a dedicated left-turn lane on Lake Washington Blvd NE has already been constructed. The applicant is proposing the addition of two lanes into Gene Coulon Park from Lake Washington Blvd NE and the signalization of the intersection. Additionally, the applicant is also proposing the installation of the railroad crossing arms at the entrance of the development. The planned improvements include channelization for turning movements to and from both Gene Coulon Park and Southport. The improvements would provide for better turning movements to and from Lake Washington Blvd NE.</i></p> <p><i>The major access point to the parking lot, which is at the southeast corner of the lot, is located as far away from the waterfront and as close to access to Lake Washington Boulevard NE as possible in order to minimize parking conflicts between hotel guests, office tenants, and residents. The internal street provides integrated access to street and parking facilities for both the hotel and the office buildings as well as a drop-off point for hotel guests, visitors, and tour groups that is separate from other uses at Southport.</i></p> <p><i>The proposed development is expected to maintain the safety and efficiency of pedestrian and vehicle circulation on the site if all conditions of approval are complied with.</i></p>
✓	<p><b>Internal Circulation:</b> Promoting safety and efficiency of the internal circulation system, including the location, design and dimensions of vehicular and pedestrian access points, drives, parking, turnarounds, walkways, bikeways, and emergency access ways.</p> <p><u>Staff Comment:</u> See discussion above under Circulation.</p>
✓	<p><b>Loading and Delivery:</b> Separating loading and delivery areas from parking and pedestrian areas.</p> <p><u>Staff Comment:</u> The loading and service area would be separated from the parking and pedestrian areas by locating off of the emergency access road along the west side of the property adjacent to the Boeing facility (Exhibit 2). Access to the service area would be provided via the ramp at the southeast corner of the parking lot and along the emergency vehicle access road along the south and west perimeter of the property. Views of the service</p>

	<p>area itself would be screened from the hotel entry area, terrace, and parking lot by a proposed planter screen wall with a trellis. The applicant would be required to provide and have approved an irrigation plan prior to construction permit approval.</p>
✓	<p><b>Transit and Bicycles:</b> Providing transit, carpools and bicycle facilities and access.</p> <p><i>Staff Comment:</i> Convenient transit service is not provided to the area so no facilities are proposed. Although no carpool spaces have been provided, stalls have been provided for up to 50 bicycles (Exhibit 3).</p>
✓	<p><b>Pedestrians:</b> Providing safe and attractive pedestrian connections between parking areas, buildings, public sidewalks and adjacent properties.</p> <p><i>Staff Comment:</i> See discussion above under Circulation.</p>
<b>h. OPEN SPACE:</b>	
✓	<p>Incorporating open spaces to serve as distinctive project focal points and to provide adequate areas for passive and active recreation by the occupants/users of the site.</p> <p><i>Staff Comment:</i> Approximately 3.2 acres of ground related opens space is planned for the entire Southport development. The hotel has two large decks located on top of the east and west wings of the 75-foot high portion of the hotel. Each deck is approximately 2,800 square feet in size and open on three sides to provide maximum sun exposure and territorial views. Both decks are accessible to hotel guests from the elevators and corridors at this level. The applicant has achieved an attractive space that is inviting and comfortable for pedestrians.</p>
<b>i. VIEWS AND PUBLIC ACCESS:</b>	
✓	<p>When possible, providing view corridors to shorelines and Mt. Rainier, and incorporating public access to shorelines.</p> <p><i>Staff Comment:</i> See discussion above under Views.</p>
<b>j. NATURAL SYSTEMS</b>	
✓	<p>Arranging project elements to protect existing natural systems where applicable.</p> <p><i>Staff Comment:</i> There are no other natural systems located on site with the exception of shorelines. The proposed hotel and public opens spaces would be sited in order to maximize shoreline access and views of Lake Washington.</p>
<b>k. SERVICES AND INFRASTRUCTURE:</b> Making available public services and facilities to accommodate the proposed use.	
✓	<p><b>Police and Fire:</b></p> <p><i>Staff Comment:</i> Police and Fire Prevention staff indicated that sufficient resources exist to furnish services to the proposed development; if the applicant provides Code required improvements and fees. The applicant would be required to pay an appropriate Fire Impact Fee. Currently this fee is assessed at \$0.56 per square foot of new building area.</p> <p>Prior to the issuance of the building permit, the applicant would be required to coordinate with the Police Department to include on-site safety features that would help lower demand for service.</p>
✓	<p><b>Parks and Recreation:</b></p> <p><i>Staff Comment:</i> The requested Hotel at Southport Site Plan is consistent with and implements the approved Southport Master Site Plan-2000 (Exhibit 14) as amended by a previous Minor Modification approved on February 4, 2008 (Exhibit 15) and requested to be modified as part</p>

	<p>of the subject application (see Table 'A').</p> <p>The subject sites proximity to Gene Coulon Park and the waterfront promenade provides an opportunity to create a public water-enjoyment or water dependent commercial space. Opportunities for passive recreation would be created through offering views of Lake Washington and access to active recreation opportunities at Gene Coulon Park.</p>
✓	<p><b>Drainage:</b></p> <p><u>Staff Comment:</u> The existing 7.99 acre site consists of 2.5 acres of impervious surface area (asphalt and gravel) and 5.49 acres of pervious area (open grassy areas). A drainage report dated April 25, 2014 has been submitted by Coughlin Porter Lundeen Engineering with the site plan application (Exhibit 8). The proposed development is subject to full drainage review in accordance with the 2009 King County Surface Water Manual and City of Renton Amendments to the KCSWM, Chapters 1 and 2.</p> <p>All core and special requirements have been discussed in the report. Based on the City's flow control map, this site falls within the Flow Control Duration Standard, Existing Conditions. The site is located within the Lower Cedar River Basin and within a ¼ mile of Lake Washington. The project is meets the Direct Discharge Exemption. New flows created by the project would discharge directly into Lake Washington through a piped system. Runoff from the new parking lot would be routed through a wetvault before discharging into Lake Washington. Additional analysis may be required at construction submittal.</p> <p>A surface water system development fee of \$0.491 per square foot of new impervious surface would apply. The fee is payable prior to issuance of the utility construction permit.</p>
✓	<p><b>Transportation:</b></p> <p><u>Staff Comment:</u> Vehicular access is provided from the closest public street, Lake Washington Boulevard NE, to the Southport development. Pedestrian access is provided along a portion of the streets adjacent to the Southport property. A continuous pedestrian system within the Southport development provides access to the waterfront and to Gene Coulon Park.</p> <p>The applicant submitted a trip generation analysis (Exhibit 6) for the requested reduction in hotel rooms/space and provides a comparison to the PM peak hour trip generation analyses documented in the Southport Master Site Plan approval (Exhibit 14) and the 2008 Southport Master Site Plan Minor Modification (Exhibit 15). The transportation assessment notes that that the PM Peak Hour trips generated by the uses in the proposed Minor Modification to the Southport Master Site Plan (1,314 trips) are less than the number of PM Peak Hour trips in the approved 2008 Minor Modification to the Master Site Plan due to a reduction in the number of hotel rooms. Additionally, the proposed number of trips is within the range of PM Peak Hour trips analyzed in the Planned Action (1,355 trips). As noted by TENW, the proposed Minor Modification to the Southport Master site Plan does require any mitigation measures beyond those required for the Level II Site Plan approved in 2000.</p> <p>As a result, no additional transportation analysis or traffic mitigation is anticipated. The measures identified in the Southport Planned Action Mitigation Document (Exhibit 17) mitigate the transportation impacts of the proposed hotel.</p> <p>The applicant is proposing the addition of two lanes into Gene Coulon Park from Lake Washington Blvd and the signalization of the intersection. Additionally, the applicant is also proposing the installation of the railroad crossing arms at the entrance of the development. The planned improvements include channelization for turning movements to and from both Gene Coulon Park and Southport. They also would provide for better turning movements to and from Lake Washington Blvd NE. However, additional queue length is needed in order to</p>

	<p>support left turns into and out of the proposed development. Therefore, staff recommends as a condition of approval the applicant be required to construct improvements at the intersection of the site access/Coulon Park entrance and Lake Washington Blvd N per the approved construction drawings contained in 2/24/14 Agreement Between City of Renton and Hotel at Southport, LLC and Southport, LLC, (CAG 14-025). In addition, the applicant would be required to extend the Northbound left turn lane on Lake Washington Blvd N at the site access/Coulon Park entrance road to provide a total queue length of 145 feet. In addition, extend the Southbound left turn lane and the combined straight/left turn lane on Lake Washington Blvd N at the NE Park Drive intersection to provide a total queue length of 150 feet. The applicant shall construct the improvements prior to temporary occupancy of the proposed hotel.</p> <p>Traffic impact fees would be determined at time of building permit(s).</p>
	<p><b>Schools:</b> Not Applicable.</p>
<p>✓</p>	<p><b>Water and Sewer:</b></p> <p><u>Staff Comment:</u> Water and sewer service would be provided by the City of Renton. A minimum of a 10-inch looped water main within the proposed perimeter fire access road along the north, west and south property lines of the subject properties would be required. The new looped water main shall connect to the existing 12-inch water main located at the northeast corner and also at the southeast corner of the subject property. Staff recommends as a condition of approval that the applicant record a 15-foot wide utility easement to be dedicated to the City for the water main and appurtenances prior to temporary occupancy approval. A sewer main extension would also be required on site.</p>
<p><b>I. PHASING:</b> The applicant is not requesting any additional phasing request.</p>	

27. The proposal requires a Shoreline Substantial Development in order to account for the approved Minor Modification to the Master Site Plan and the proposed Minor Modification to the Master Site Plan (see Table A). The following table contains project elements intended to comply with the vested Shoreline Master Program Regulations, as outlined in RMC 4-3-090 (Exhibit 11):

<p><b>Table C: Vested Shoreline Master Program Regulations</b></p>	
<p>The subject site is located in the <b>Urban Environment</b>. The objective of the Urban Environment is to ensure optimum utilization of shorelines within urbanized areas by providing public use, especially access to and along the water's edge and by managing development so that it enhances and maintains shorelines for a multiplicity of viable and necessary urban uses.</p>	
<p><b>N/A Compliant with original Shoreline Substantial Development Permit</b></p>	<p><b>Use Regulations in the Urban Environment:</b> Because shorelines suitable for urban uses are a limited resource, emphasis shall be given to development within already developed areas and particularly to water oriented industrial and commercial areas. Priority is also given to public visual and physical access to water in the Urban Environment. Identifying needs and planning for the acquisition of urban land for permanent public access to the water in the Urban Environment shall be accomplished through the Master Program (1999). To enhance waterfront and ensure maximum public use, industrial and commercial facilities shall be designed to permit pedestrian waterfront activities where practicable, and the various access points ought to be linked to non-motorized transportation routes such as bicycle and hiking paths.</p> <p><u>Staff Comment:</u> The proposal provides both commercial and residential uses in a mixed use format that is consistent with recently developed surrounding properties. A public enjoyment or water dependent commercial use, such as a hotel, was found to be</p>

	<p><i>compatible, as part of the approved Shoreline Substantial Development Permit (Exhibit 22), with the other uses within the Southport development, the adjacent Boeing facility, the adjacent park, and the public waterfront promenade which abuts the hotel site. The restaurant and convention space would complement the existing uses in the area by drawing people from Gene Coulon Park or adjacent Boeing facility to the pedestrian promenade.</i></p> <p><i>A hotel would create increased activity on the promenade and would serve as a signal that the space is accessible by the public. The proposed hotel would complement the existing on site and adjacent uses.</i></p>
<p><b>Environmental Effects</b></p>	
<p><b>N/A Compliant with original Shoreline Substantial Development Permit</b></p>	<p><b>Pollution and Ecological Disruption:</b> The potential effects on water quality, water, and land vegetation, water life, and other wildlife (including, for example spawning areas, migration and circulation habits, natural habitats, and feeding), soil quality and all other environmental aspects must be considered in the design plans for any activity or facility which may have detrimental effects on the environment:</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for proposed hotel considered environmental aspects into its design (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with and implementation of the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the pollution and ecological disruption.</i></p>
<p><b>N/A Compliant with original Shoreline Substantial Development Permit</b></p>	<p><b>Erosion:</b> Erosion is to be controlled through the use of vegetation rather than structural means where feasible.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered the control of erosion through vegetation into its design (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the erosion.</i></p> <p><i>The existing waterfront consists of a concrete bulkhead across the entire site frontage along Lake Washington which would not be altered. The depth of the water at the bulkhead varies from approximately 6 feet at the eastern end to approximately 14 feet at the western end and the top of the bulkhead is 2 to 3 feet above the ordinary high water mark. No work is proposed below the ordinary high water mark of Lake Washington.</i></p>
<p><b>Compliant if all conditions of approval are met</b></p>	<p><b>Geology:</b> Important geological factors such as possible slide areas, on a site must be considered. Whatever activity is planned under the application for the development permit must be safe and appropriate in view of the geological factors prevailing.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered geological factors into its design (Exhibit 13). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the geology.</i></p> <p><i>A Geotechnical Report prepared by HartCrowser, dated April 18, 2014 was submitted as part of the Site Plan Review application (Exhibit 9). The report states there is a risk of liquefaction during a significant seismic event through the upper 20-50 feet below grade. There are two potential foundation support alternatives. The first alternative is driven piles (precast or driven grout) however the report states this alternative would not address site</i></p>

	<p><i>liquefaction potential. The second alternative is to complete ground improvement using stone columns (Geopiers) across the entire site and construct spread footings.</i></p> <p><i>The geotechnical report includes specific recommendations in order to mitigate potential geotechnical impacts including: ground improvement verification. Therefore, staff recommends as a condition of approval the applicant be required to comply with the recommendations included on page 12 of the provided Geotechnical Engineering Report (Exhibit 7).</i></p>
<p><b>Use Compatibility:</b> The potential impact of any of the following on adjacent, nearby, and possibly distant land and shoreline users shall be considered in the design plans and efforts made to avoid or minimize detrimental aspects:</p>	
	<p><b>View Obstruction:</b> Buildings, smokestacks, machinery, fences, piers, poles, wires, signs, lights, and other structures.</p> <p><i>Staff Comment: The proposed design is more consistent with the vested Shoreline Master Program than the hotel design previously approved as part of the 2000 Substantial Shoreline Development Permit. The hotel would have a much lower presence along the waterfront and less of a view impact than the conceptual hotel envisioned in the 2000 Shoreline Substantial Development Permit. Instead of being a continuous 75 foot tall structure with 10 foot setbacks above the fifth story, a large terrace along the north frontage would be located approximately 28 feet above ground-level and occupy only 50% of the hotel's waterfront frontage (Exhibit 5). The C-shaped portion of the hotel between 50 and 100 feet of the shoreline has two 75-foot high wings separated by a large central terrace also approximately 28 feet above ground-level and topped by two large, 50 foot deep rooftop terraces. The terraces provide visual access to the waterfront and territorial views. The wings of the 125-foot tall portion of the C-shaped building are set back 100 feet from the shoreline as permitted by the COR-3 zone while the central portion is set back approximately 145 feet from the shoreline. The 125 foot high portion of the hotel is oriented to an internal street shared with the office buildings and not to the shoreline side of the site.</i></p> <p><i>The applicant has submitted revised view impacts studies from various points in the City (Exhibit 19). Views depicting the redeveloped site from Mercer Island and north of Gene Coulon park were not provided due to the distance from the site. Other than the residential uses constructed as part of the Southport Master Site Plan the proposal is not adjacent to residential uses or residential zoned property. While it is likely that views from areas of the City that are at higher elevations would be affected there would not be any views from residential areas or public areas obstructed.</i></p> <p><i>The three approved office buildings to the south of the hotel site (each 125 feet in height) impede views of the hotel, and of the shoreline, from Park Avenue. The Bristol 1 residential building prevents views from the playground in Gene Coulon Park. Trees would obstruct views of the development from Jones Avenue NE at NE 14<sup>th</sup> St and the hotel would remain essentially indistinguishable from the tall office buildings in the view from the dock at Gene Coulon Park. Overall, the views from nearby surrounding areas are the same as for the approved Substantial Shoreline Development.</i></p>
<p><b>N/A Compliant with original Shoreline Substantial Development</b></p>	<p><b>Community Disturbances:</b> Noise, odors, night lighting, water and land traffic, and other structures and activities.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel provided mitigation for community disturbances (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial</i></p>

<p><b>Permit</b></p>	<p><i>Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to community disturbances.</i></p> <p><i>See detailed discussion within Table B: Site Plan Review.</i></p>
<p>✓</p>	<p><b>Design Theme:</b> Architectural styles, exterior designs, landscaping patterns, and other aspects of the overall design of a site shall be uniform or coordinated design, planned for the purpose of visual enhancement as well as for serving a useful purpose.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel provided a uniform and coordinated design (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the design theme.</i></p> <p><i>See detailed discussion within Table B: Site Plan Review.</i></p>
<p><b>N/A Compliant with original Shoreline Substantial Development Permit</b></p>	<p><b>Visually Unpleasant Areas:</b> Landscaped screening shall be used to hide from public view any area that may impinge upon the visual quality of a site, for example, disposal bins, storage yards, and outdoor work areas.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel provided screened visually unpleasant areas (Exhibit 13). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 13) as it relates to visually unpleasant areas.</i></p> <p><i>See detailed discussion within Table B: Screening.</i></p>
<p><b>N/A Compliant with original Shoreline Substantial Development Permit</b></p>	<p><b>Outdoor Activities:</b> Work areas, storage, and other activities on a site in a residential area shall be in enclosed buildings, as reasonably possible, to reduce distractions and other effects on surrounding areas. Outdoor activities of commercial and industrial operations shall be limited to those necessary for the operation of the enterprise. Outdoor areas shall not be used for storage of more than minimal amounts of equipment, parts, materials, products, or other objects.</p> <p><i>Staff Comment: Not applicable.</i></p>
<p><b>Public Access:</b> Where possible, space and right-of-way shall be left available on the immediate shoreline so that trails, non-motorized bike paths, and/or other means of public use may be developed providing greater shoreline utilization. Any trail system shall be designed to avoid conflict with private residential property rights. No property shall be acquired for public use without just compensation to the owner.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered public access into its design (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to public access connections.</i></p> <p><i>The increase in height should not interfere with the public use of the shoreline. The hotel would be setback 50 feet. The public use of the shoreline would be enhanced by providing increased services, such as a restaurant, convention space, and a hotel. Staff has recommended the applicant record a public recreation easement to provide the final trail connection from Gene Coulon Park to the Cedar River Trail (see discussion within Table B: Circulation).</i></p>	
<p><b>Facility Arrangement:</b> Where feasible shoreline developments shall locate the water dependent, water-related and water-enjoyment portions of their developments along the shoreline and place all other</p>	

facilities inland.

*Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered facility arrangement into its design (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the arrangement of facilities on the site.*

*Hotel functions along the north side of the building are located at grade with the waterfront promenade. Functions include exercise rooms, spa and a pool in the east wing; pre-function areas in the central section; and a large 2-level ballroom in the west wing. The ballroom has doors and large windows on the north façade providing direct physical and visual access to the promenade and Lake Washington. The south half of this level is devoted to hotel administrative and back of house functions.*

**Landscaping:** The natural and proposed landscaping should be representative of the indigenous character of the specific types of waterway (stream, lake edge, marshland) and shall be compatible with the Northwest image. The scenic, aesthetic, and ecological qualities of natural and developed shorelines should be recognized and preserved as valuable resources.

*Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered the scenic, aesthetic, and ecological qualities of natural and developed shorelines in its design (Exhibit 13). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 13) as it relates to landscaping.*

*See discussion within Table B: Landscaping.*

**Unique and Fragile Areas:** Unique features and wildlife habitats should be preserved and incorporated into the site. Fragile areas shall be protected from development and encroachment.

*Staff Comment: Not applicable.*

**Specific Use Regulations for Commercial Developments:**

<p><b>N/A</b>  <b>Compliant</b>  <b>with original</b>  <b>Shoreline</b>  <b>Substantial</b>  <b>Development</b>  <b>Permit</b></p>	<p><b>Location of Developments:</b> New commercial developments are to be encouraged to locate in those areas where current commercial uses exist. New commercial developments on Lake Washington which are neither water-dependent, nor water related, nor water enjoyment, no which do not provide significant public access to and along the water's edge will not be permitted upon the shoreline.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel considered the location of new commercial developments (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to the location of new commercial developments.</i></p> <p><i>See discussion within Table B: Site Plan Review.</i></p>
<p><b>N/A</b>  <b>Compliant</b>  <b>with original</b>  <b>Shoreline</b>  <b>Substantial</b>  <b>Development</b>  <b>Permit</b></p>	<p><b>Incorporation of Public Recreational Opportunities:</b> Commercial developments should incorporate recreational opportunities along the shoreline for the general public.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel incorporated public recreation opportunities (Exhibit 13). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, does not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 13) as it relates to the incorporation of public recreation</i></p>

	<p><i>opportunities.</i></p> <p><i>The subject sites proximity to Coulon Park and the waterfront promenade continue to provide an opportunity to create a public water-enjoyment or water dependent commercial space that could enable people to enjoy views of Lake Washington.</i></p>
✓	<p><b>View Impacts:</b> The applicant for a shoreline development permit for a new commercial development must indicate in his application the effect which the proposed commercial development will have upon the scenic view prevailing in the given area. Specifically, the applicant must state in his permit what steps have been taken in the design of the proposed commercial development to reduce to a minimum interference with the scenic view enjoyed by any significant number of people in the area.</p> <p><i>Staff Comment: See discussion above under View Obstruction.</i></p>
N/A Compliant with original Shoreline Substantial Development Permit	<p><b>Setback:</b> A commercial building should be located no closer than 50 feet to the ordinary high water mark.</p> <p><i>Staff Comment: The City concluded that the approved Substantial Shoreline Development Permit for the proposed hotel met commercial setback along shorelines (Exhibit 22). The proposed increase in hotel height, necessitating the additional Shoreline Substantial Development Permit, would not prevent consistency with the approved Shoreline Substantial Development Permit (Exhibit 22) as it relates to setbacks.</i></p> <p><i>See discussion within Table B: Shoreline Setback.</i></p>

**G. CONCLUSIONS:**

1. The Southport Hotel is consistent with the Minor Modification criteria to a Master Site Plan and therefore complies with the Planned Action thresholds.
2. The proposal complies with the Administrative Site Plan Review Criteria if all conditions of approval are met.
3. The proposal is consistent with the requested Shoreline Substantial Development Permit and would not prevent consistency with approved Shoreline Substantial Development Permit (Exhibit 22).
4. This requested permit is granted pursuant to the Shoreline Management Act of 1971.
5. The issuance of a license under the Shoreline Management Act of 1971 shall not release the applicant from compliance with federal, state, and other permit requirements.
6. This Shoreline Substantial Development permit may be rescinded pursuant to Section 14(7) of the Shoreline Management Act of 1971 in the event the applicant fails to comply with any condition(s) hereof.
7. Construction permits shall not be issued until twenty-one (21) days after approval by the Washington State Department of Ecology or until any review proceedings initiated within this twenty-one (21) day review period have been completed.
8. The proposal is compliant and consistent with the plans, policies, regulations and approvals to which it is vested if all conditions of approval are met.

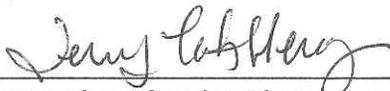
9. Staff does not anticipate any adverse impacts on surrounding properties and uses as long as the conditions of approval are complied with.
10. The proposed use is anticipated to be compatible with existing and future surrounding uses as permitted in the UCN-2 zoning classification.
11. The scale, height and bulk of the proposed structure is appropriate for the site.
12. The proposed hotel offers a superior design solution as an alternative to the upper story setback requirement of the zone.
13. Safe and efficient access and circulation has been provided for all users if all conditions of approval are met.
14. The proposed structure would not block approved view corridors to shorelines or Mt. Rainier.
15. There are adequate public services and facilities to accommodate the proposed use.
16. Adequate parking for the proposed use has been provided and the proposal is compliant with all parking modification criteria if all conditions of approval are complied with.
17. The proposed site plan ensures safe movement for vehicles and pedestrians and has mitigated potential effects on the surrounding area if all conditions of approval are complied with.
18. The proposed development would not generate any long term harmful or unhealthy conditions. Potential noise, light and glare impacts from the proposed use have been evaluated and mitigated if all conditions of approval are complied with.
19. Landscaping has been provided in all areas not occupied by the building or paving.

#### **H. DECISION:**

The proposed Master Site Plan Minor Modification, Administrative Site Plan, Shoreline Substantial Development Permit, Upper Story Setback Modification, and (stall length) Parking Modification for the Hotel at Southport, File No. LUA14-000645, SM, SA-A, MOD, MOD are **approved**. The Parking Modification (for an increase in number of stalls) is **denied**. The proposal is subject to the following conditions:

1. The applicant shall comply with all mitigation measures issued as part of the SFEIS, dated September 17, 1999 (Exhibit 17).
2. The applicant shall be required to demonstrate compliance with the required average western property line setback of 20 feet prior to building permit approval.
3. The applicant shall submit screening detail for all surface mounted utility and mechanical equipment to the satisfaction of the Current Planning Project Manager prior to primary building permit approval.
4. The applicant shall be required to revise the site to eliminate at least 20 parking stalls and include additional intervening landscaping within the surface parking area to the satisfaction of the Current Planning Project Manager. The revised site and landscaping plan shall be submitted to and approved by the Current Planning Project Manager prior to construction permit approval.
5. The applicant shall provide a material/colors board to the Current Planning Project Manager prior to the primary building permit approval. The materials board shall demonstrate compatibility with the Bristol Apartments and future office buildings.

6. The applicant shall submit a revised and detailed landscape plan complying with the following: a mixture of trees, shrubs, and groundcover. The revised detailed landscape plan, shall be submitted to, and approved by, the Current Planning Project Manager prior to construction permit issuance.
7. The applicant shall be required to submit a noise impact study related to potential HVAC equipment noise prior to primary building permit approval. If studies indicate that HVAC equipment noise would adversely impact residential development on the Southport site, mitigation can implemented consisting of noise barriers placed around the HVAC units, selection of alternative equipment with reduced noise levels, or provision of silencers for air intake and exhaust.
8. The applicant shall be required to extend the Northbound left turn lane on Lake Washington Blvd N at the site access/Coulon Park entrance road to provide a total queue length of 145 feet. In addition, extend the Southbound left turn lane and the combined straight/left turn lane on Lake Washington Blvd N at the NE Park Drive intersection to provide a total queue length of 150 feet. The applicant shall construct the improvements prior to temporary occupancy of the proposed hotel.
9. The applicant shall comply with additional recommendations included on page 12 of the provided Geotechnical Engineering Report (Exhibit 7).



**Jennifer Henning, Planning Director**

9-23-2014

**Date**

TRANSMITTED this 23<sup>rd</sup> day of September 2014 to the Contact/Applicant/Owner:

**Applicant:**

Greg Krape  
Hotel at Southport, LLC  
1083 Lake Washington Blvd, Suite 50  
Renton, WA 98056

**Owner:**

Michael Christ  
1083 Lake Washington Blvd, Suite 50  
Renton, WA 98056

**Contact:**

Bill Stalzer  
Stalzer & Associates  
603 Stewart St, Suite 512  
Seattle, WA 98101

TRANSMITTED this 23<sup>rd</sup> day of September 2014 to the Parties of Record:

Yeh-Hee Hahn  
CBRE Brokerage Services  
1201 Pacific Ave, Ste #1502  
Tacoma, WA 98042  
The Boeing Company  
PO Box 3707  
Seattle, WA 98124-2207

Bob Braaf  
1085 Lake Washington Blvd N, Ste  
#C411  
Renton, WA 98056

Mike O'Halloran  
4420 4<sup>th</sup> Ave  
Renton, WA 98059

TRANSMITTED this 23<sup>rd</sup> day of September 2014 to the following:

C.E. "Chip" Vincent, CED Administrator  
Steve Lee, Development Engineering Manager  
Vanessa Dolbee, Current Planning Manager  
Fire Marshal

**Land Use Action Appeals, Request for Reconsideration, & Expiration**

The review decisions will become final if the decisions are not appealed within 14 days of the decision date.

**Minor Modification to Approved Master Site Plan Approval Appeal: Appeals of the administrative modification to approved master site plan must be filed in writing to the Hearing Examiner on or before 5:00 p.m. on October 7, 2014.**

**Administrative Site Development Plan Approval Appeal: Appeals of the administrative site development plan review decision must be filed in writing to the Hearing Examiner on or before 5:00 p.m. on October 7, 2014.**

**Shoreline Substantial Development Permit: Appeals of the administrative site development plan review decision must be filed in writing to the Hearing Examiner on or before 5:00 p.m. on October 7, 2014.**

**Parking Modification Approval/Denial Appeal: Appeals of the parking modification decisions must be filed in writing to the Hearing Examiner on or before 5:00 p.m. on October 7, 2014.**

**Upper Story Modification Approval Appeal: Appeals of the parking modification decisions must be filed in writing to the Hearing Examiner on or before 5:00 p.m. on October 7, 2014.**

**SHORELINE APPEALS:** Appeals of Shoreline Substantial Development Permit issuance must be made directly to the Shorelines Hearings Board. Appeals are made by filing a request in writing within the twenty-one (21) days of receipt of the final order and concurrently filing copies of such request with the Washington State Department of Ecology and the Attorney General's office as provided in section 18(1) of the Shorelines Management Act of 1971. All copies of appeal notices shall also be filed with the City of Renton Planning Division and the City Clerk's office.

**ALL OTHER APPEALS:** An appeal of the decision(s) must be filed within the 14-day appeal period (RCW 43.21.C.075(3); WAC 197-11-680). Renton Municipal Code Section 4-8-110 governs appeals to the Hearing Examiner. Appeals must be filed in writing together with the \$250.00 application fee to Hearing Examiner, City of Renton, 1055 South Grady Way, Renton, WA 98057. Additional information regarding the appeal process may be obtained from the City Clerk's Office, Renton City Hall - 7th Floor, (425) 430-6510.

**RECONSIDERATION:** Within 14 days of the decision date, any party may request that a decision be reopened by the Administrator (Decision-maker). The Administrator (Decision-maker) may modify his decision if material evidence not readily discoverable prior to the original decision is found or if he finds there was misrepresentation of fact. After review of the reconsideration request, if the Administrator (Decision-maker) finds sufficient evidence to amend the original decision, there will be no further extension of the appeal period. Any person wishing to take further action must file a formal appeal within the 14-day appeal timeframe.

**SHORELINE EXPIRATION PERIOD:** Unless a different time period is specified in the shoreline permit as authorized by RCW 90.58.143 and subsection J1 of RMC 4-9-190, construction activities, or a use or activity, for which a permit has been granted pursuant to this Master Program must be commenced within two (2) years of the effective date of a shoreline permit, or the shoreline permit shall terminate, and a new permit shall be necessary. However, the Planning Division may authorize a single extension for a period not to exceed one year based on reasonable factors, if a request for extension has been filed with the Planning Division before the expiration date, and notice of the proposed extension is given to parties of record and the Washington State Department of Ecology. **DEFINITION OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES:** the construction applications must be submitted, permits must be issued, and foundation inspections must be completed before the end of the two (2) year period.

**ALL OTHER EXPIRATION PERIODS:** The Administrative Site Development Plan Review decision will expire two (2) years from the date of decision. A single two (2) year extension may be requested pursuant to RMC 4-9-200.

**THE APPEARANCE OF FAIRNESS DOCTRINE:** provides that no ex parte (private one-on-one) communications may occur concerning the land use decision. The Doctrine applies not only to the initial decision, but to Appeals

to the Hearing Examiner as well. All communications after the decision/approval date must be made in writing through the Hearing Examiner. All communications are public record and this permits all interested parties to know the contents of the communication and would allow them to openly rebut the evidence in writing. Any violation of this doctrine could result in the invalidation of the appeal by the Court.

**ADVISORY NOTES TO APPLICANT**

The following notes are supplemental information provided in conjunction with the administrative land use action. *Because these notes are provided as information only, they are not subject to the appeal process for the land use actions.*

Planning:

1. RMC section 4-4-030.C.2 limits haul hours between 8:30 am to 3:30 pm, Monday through Friday unless otherwise approved by the Development Services Division.
2. Within thirty (30) days of completion of grading work, the applicant shall hydroseed or plant an appropriate ground cover over any portion of the site that is graded or cleared of vegetation and where no further construction work will occur within ninety (90) days. Alternative measures such as mulch, sodding, or plastic covering as specified in the current King County Surface Water Management Design Manual as adopted by the City of Renton may be proposed between the dates of November 1st and March 31st of each year. The Development Services Division's approval of this work is required prior to final inspection and approval of the permit.
3. Commercial, multi-family, new single-family and other nonresidential construction activities shall be restricted to the hours between seven o'clock (7:00) a.m. and eight o'clock (8:00) p.m., Monday through Friday. Work on Saturdays shall be restricted to the hours between nine o'clock (9:00) a.m. and eight o'clock (8:00) p.m. No work shall be permitted on Sundays.
4. Pile driving (if applicable) shall be restricted to the hours between 7:00 am and 8:00 pm, Monday through Friday. No pile driving work shall be permitted on Saturdays and Sundays. Or the City of Renton may authorize alternate construction hours for reasonable cause, consistent with City regulations where applicable.
5. As part of the building permit design process, a geotechnical engineer retained by the developer shall survey existing structures in the surrounding area to determine the potential for vibration related impacts.
6. All landscaping shall be irrigated by an approved irrigation system prior to final occupancy permits
7. Directional lighting and shading provisions for all light standards along the Lake Washington promenade is required to be implemented.

Water:

1. A minimum of a 10-inch looped water main within the proposed perimeter fire access road along the north, west and south property lines of the subject properties will be required. The new looped water main shall connect to the existing 12-inch water main located at the northeast corner and also at the southeast corner of the subject properties. A 15-foot wide utility easement shall be dedicated to the City for the water main and appurtenances.
2. Installation of new fire hydrants will be required by Renton Fire Prevention. The number and location of the hydrants shall be determined based on the City's review of the site utilities and roadway plan.
3. Installation of fire sprinkler stub with a detector double check valve assembly (DDCVA) for backflow prevention will be required. The DDCVA shall be installed in an outside underground vault or in the building sprinkler room if it meets the conditions as shown on City's standard plan for the interior installation of a DDCVA.
4. Domestic water meter with a reduced backflow prevention assembly (RPBA) will be required. The RPBA shall be installed behind the meter and inside a heated enclosure ("hot-box") per City standard plan no. 350.2. Sizing of the meter shall be done in accordance with the Uniform Plumbing Code meter sizing criteria. Meter size 3-inch and above shall be installed inside a concrete vault located outside of the building per City Standard Plan No. 320.4. The applicant/design engineer shall provide adequate room in the parking lot or in the landscape areas for the installation of the water meter(s) vault and fire sprinkler vault.
5. Installation of landscape irrigation meter and double check valve assembly (DCVA) may be required.
6. System development fees for water will be owed based on the size of the new domestic water meter, fire line and landscape irrigation meter that will serve the new building. Meter installation fees will be based on the size of each new meter.

Sewer.

1. A sewer main extension will be required on site.
2. System development fees for sewer will be owed based on the size of the new domestic water meter(s) that will serve the new building. Meter installation fees will be based on the size of each new meter.
3. Grease interceptor shall be sized in accordance with the UPC and shall be reviewed and approved by the City's waste

water department.

Surface Water:

1. A surface water system development fee of \$0.491 per square foot of new impervious surface will apply. This is payable prior to issuance of the utility construction permit.
2. A drainage report dated April 25, 2014 has been submitted by Coughlin Porter Lundeen Engineering with the site plan application. The proposed development is subject to Full Drainage Review in accordance with the 2009 King County Surface Water Manual and City of Renton Amendments to the KCSWM, Chapters 1 and 2. The existing 7.99 acre site consists of 2.5 acres of impervious surface area (asphalt and gravel) and 5.49 acres of pervious area (open grassy areas). The engineer performed a Level 1 Analysis. All core and special requirements have been discussed in the report. Based on the City's flow control map, this site falls within the Flow Control Duration Standard, Existing Conditions. The site is located within the Lower Cedar River Basin and within a ¼ mile of Lake Washington. The project meets the Direct Discharge Exemption since the new flows created by the project will discharge directly into Lake Washington through a pipe system. Runoff from the new parking lot will be routed through a wetvault before discharging into Lake Washington. Additional analysis may be required at construction submittal.
3. A Construction Stormwater General Permit from Department of Ecology will be required if grading and clearing of the site exceeds one acre. A Stormwater Pollution Prevention Plan (SWPPP) is required for this site.
4. Paving and trench restoration will comply with the City's Trench Restoration and Overlay Requirements.

Transportation:

1. Traffic impact fees will be determined at time of building permit.

Fire:

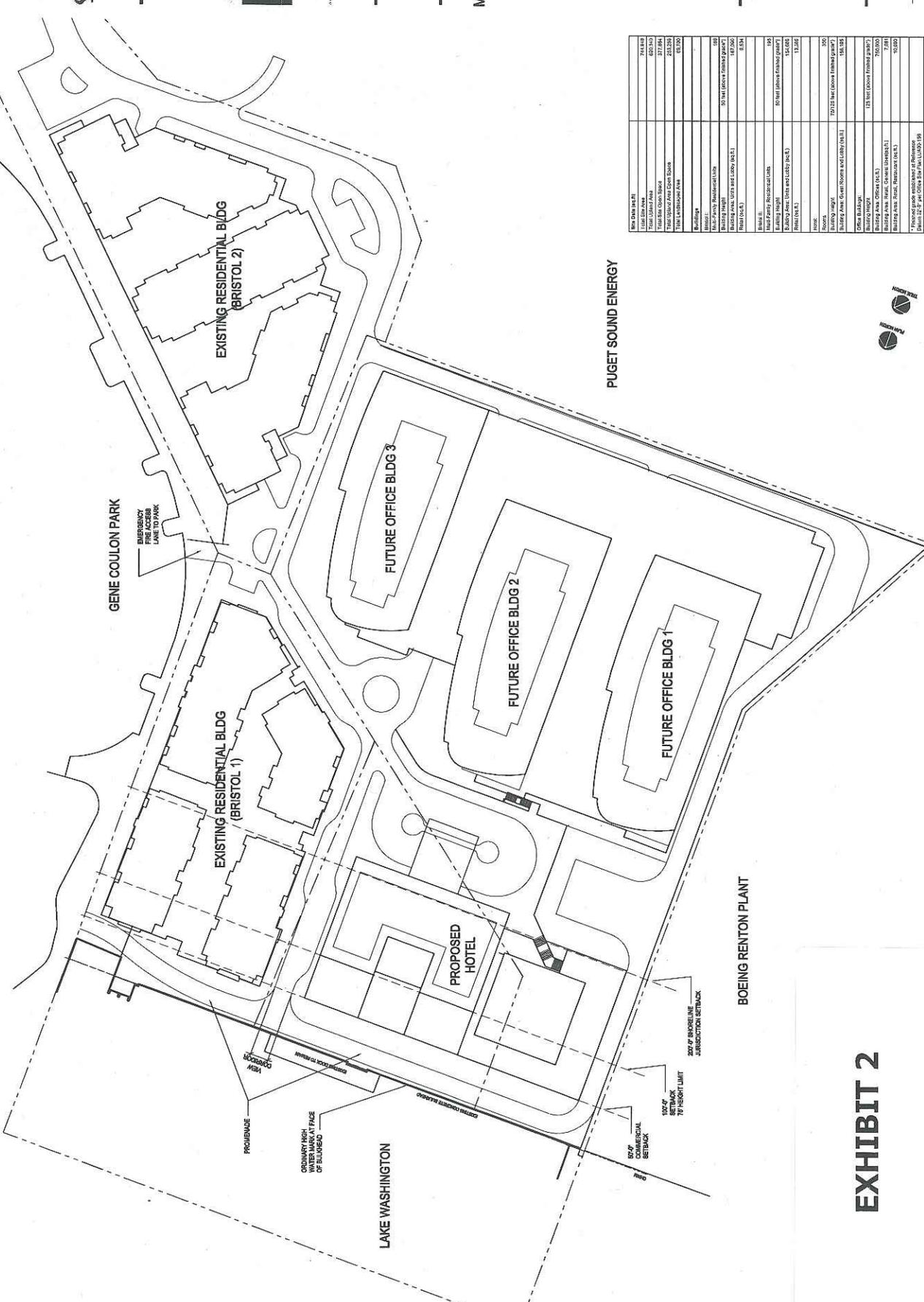
1. The preliminary fire flow is 3,000 gpm. A minimum of one fire hydrant is required within 150-feet of the structure and two additional hydrants are required within 300-feet of the structure. A looped water main is required to be installed around the building and the site, maximum hydrant spacing in commercial areas is 300-feet on center. A minimum of a 12-inch main shall be extended throughout the site to accommodate future larger high rise office buildings. Off-site improvements required as well.
2. Approved fire sprinkler, fire standpipe and fire alarm systems are required throughout the building. Separate plans and permits required by the fire department. Direct outside access is required to the fire sprinkler riser room. Fully addressable and full detection is required for the fire alarm system.
3. Fire department apparatus access roadways are required within 150-feet of all points on the building. Fire lane signage required for the on-site roadway. Required turning radius is 25-feet inside and 45-feet outside. Roadways shall be a minimum of 20-feet wide. Roadways shall support a minimum of a 30-ton vehicle and 322-psi point loading. Minimum vertical clearance is 13-feet, 6-inches. Fire lane signage required per code. Maximum slope is 15 percent.
4. An electronic site plan is required prior to occupancy for pre-fire planning purposes.
5. All buildings equipped with an elevator in the City of Renton are required to have at least one elevator meet the size requirements for a bariatric size stretcher. Car size shall accommodate a minimum of a 40-inch by 84-inch stretcher.
6. The building shall comply with the City of Renton Emergency Radio Coverage ordinance. Testing shall verify both incoming and outgoing minimum emergency radio signal coverage. If inadequate, the building shall be enhanced with amplification equipment in order to meet minimum coverage. Separate plans and permits are required for any proposed amplification systems.
7. Applicant shall provide a completed Hazardous Material Inventory Statement for the swimming pool area and generator fuel supply prior to building permit issuance. Use of City of Renton form or approved equivalent is required.
8. Separate plans and permits required for all kitchen hood fire suppression systems.
9. The project shall comply with all applicable high rise requirements of the International Building and Fire Codes, 2012 editions.
10. All high-rise buildings shall be equipped with an approved rescue air replenishment system. The system shall provide an adequate pressurized fresh air supply through a permanent piping system for the replenishment of portable life sustaining air equipment carried by Fire and Emergency Services Department, rescue and other personnel in the performance of their duties. Location of access stations, as well as installation and maintenance of the air replenishment systems, shall meet the requirements as determined by the Fire Code Official. A specifications document for the construction of air replenishing systems that conforms to the breathing equipment used by the Renton Fire and Emergency Services Department will be made available by the Fire Code Official.

General :

1. Separate permits and fees for side sewer connection and storm connection will be required.
2. All construction utility permits for drainage and street improvements will require separate plan submittals. All utility plans shall conform to the Renton Drafting Standards. A licensed Civil Engineer shall prepare the civil plans.
3. Rockeries or retaining walls greater than 4 feet in height will be require a separate building permit. Structural

- calculations and plans shall be submitted for review by a licensed engineer. Special Inspection is required.
4. A tree removal and tree retention/protection plan shall be included with the civil plan submittal.

1 2 3 4 5 6 7 8 9



SKK&B DEVELOPMENT, INC.  
 1500 10TH AVENUE, SUITE 100  
 SEASIDE, WA 98148  
 PHONE: (206) 441-1100  
 FAX: (206) 441-1100  
 WWW.SKK&B.COM

SOUTHPORT 2014  
 MASTER PLAN MINOR  
 MODIFICATION

05.16.2014

Item	Description	Quantity	Unit	Notes
1	Site Area (Ac.)	174.48	Ac.	
2	Lot Area (Ac.)	855.94	Ac.	
3	Impervious Area (Sq. Ft.)	233,285	Sq. Ft.	
4	Impervious Area (Ac.)	5.35	Ac.	
5	Impervious Area (Sq. Ft.)	13,242	Sq. Ft.	
6	Impervious Area (Ac.)	0.30	Ac.	
7	Impervious Area (Sq. Ft.)	6,824	Sq. Ft.	
8	Impervious Area (Ac.)	0.16	Ac.	
9	Impervious Area (Sq. Ft.)	3,571	Sq. Ft.	
10	Impervious Area (Ac.)	0.08	Ac.	
11	Impervious Area (Sq. Ft.)	1,788	Sq. Ft.	
12	Impervious Area (Ac.)	0.04	Ac.	
13	Impervious Area (Sq. Ft.)	9,592	Sq. Ft.	
14	Impervious Area (Ac.)	0.22	Ac.	
15	Impervious Area (Sq. Ft.)	4,824	Sq. Ft.	
16	Impervious Area (Ac.)	0.11	Ac.	
17	Impervious Area (Sq. Ft.)	2,412	Sq. Ft.	
18	Impervious Area (Ac.)	0.05	Ac.	
19	Impervious Area (Sq. Ft.)	1,206	Sq. Ft.	
20	Impervious Area (Ac.)	0.02	Ac.	
21	Impervious Area (Sq. Ft.)	603	Sq. Ft.	
22	Impervious Area (Ac.)	0.01	Ac.	
23	Impervious Area (Sq. Ft.)	301	Sq. Ft.	
24	Impervious Area (Ac.)	0.00	Ac.	
25	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
26	Impervious Area (Ac.)	0.00	Ac.	
27	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
28	Impervious Area (Ac.)	0.00	Ac.	
29	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
30	Impervious Area (Ac.)	0.00	Ac.	
31	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
32	Impervious Area (Ac.)	0.00	Ac.	
33	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
34	Impervious Area (Ac.)	0.00	Ac.	
35	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
36	Impervious Area (Ac.)	0.00	Ac.	
37	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
38	Impervious Area (Ac.)	0.00	Ac.	
39	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
40	Impervious Area (Ac.)	0.00	Ac.	
41	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
42	Impervious Area (Ac.)	0.00	Ac.	
43	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
44	Impervious Area (Ac.)	0.00	Ac.	
45	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
46	Impervious Area (Ac.)	0.00	Ac.	
47	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
48	Impervious Area (Ac.)	0.00	Ac.	
49	Impervious Area (Sq. Ft.)	0	Sq. Ft.	
50	Impervious Area (Ac.)	0.00	Ac.	

PUGET SOUND ENERGY

# EXHIBIT 2

SOUTHPORT 2014 MASTER PLAN MINOR MODIFICATION

SCALE: 1"=40'



PKA DAVENPORT  
 DRAWN BY: WHITE



NEIGHBORHOOD DETAIL MAP

SITE INFORMATION

CONSTRUCTION TYPE: B  
OCCUPANCIES: R1, A, B, S

SITE AREA  
LOT 1 187,400 SF  
LOT 4 298,821 SF  
TOTAL 486,221 SF

BUILDING FOOTPRINT: HOTEL: 75,670 SF  
LOT COVERAGE PCT: 15.6%

Level	Area	Area
LEVEL 1	75,670 SF	
LEVEL 2	29,642 SF	5,712 SF
LEVEL 3	31,615 SF	
LEVEL 4	24,170 SF	10,753 SF
LEVEL 5	24,580 SF	10,837 SF
LEVEL 6	24,580 SF	10,837 SF
LEVEL 7	24,580 SF	10,837 SF
LEVEL 8	16,006 SF	13,952 SF
LEVEL 9	18,006 SF	14,301 SF
LEVEL 10	18,006 SF	14,301 SF
LEVEL 11	18,006 SF	14,301 SF
LEVEL 12	18,006 SF	14,437 SF
TOTAL	320,380 SF	100,105 SF

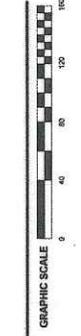
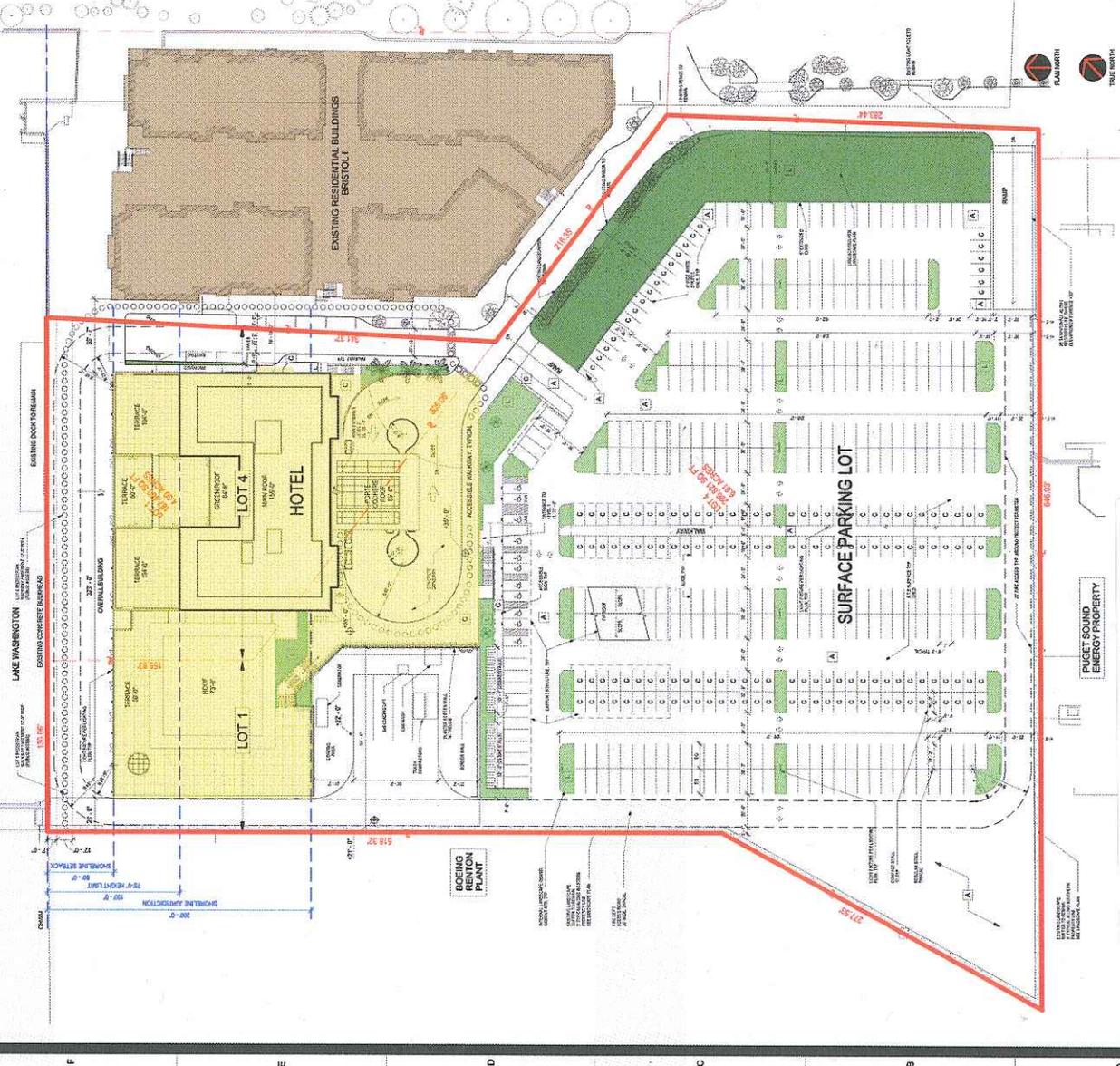
INCLUDED: HOTEL, GUEST RECEPTION, GYM, GYM LEVEL 1

Use / Function	Quantity	Unit	Factor	Subtotal
HOTEL - GUEST ROOMS	350	KEY	1.0	350
EMPLOYEES	175	PERSON	2/3	117
TOTAL PARKING SPACE REQUIRED				467

Parking - Description	Quantity	Percentage (%)
- STANDARD (8' X 18')	318	68.3%
- COMPACT (6.5' X 16')	156	32.4%
- VAN ACCESSIBLE	9	1.8%
- TOTAL PARKING	483	100.0%

Minimum Required	Quantity	Proposed
MINIMUM REQUIRED	401,500	0
ACCESSIBLE PARKING	11	11
BICYCLE PARKING	60	60
TOTAL BICYCLE PARKING STALL PROVIDED	60	60
SIZE OF BICYCLE STALL	2' X 6'	2' X 6'
COMPACTOR/TRASH	645 SF	645 SF

- LEGEND
- CONCRETE
  - ASPHALT TREATED BASE (ATB)
  - LANDSCAPE AREA (PER LANDSCAPE PLAN)
  - ACCESSIBLE PATH
- NOTES:
- SEE LANDSCAPE PLAN FOR LANDSCAPING
  - SEE SITE PLAN FOR UTILITY LOCATIONS
  - SEE CIVIL DRAWINGS FOR UTILITIES, SWANNA, & DRAINAGE CONTROL.



SITE SCALE: 1" = 100'

EXHIBIT 3



RENTON,  
WASHINGTON



SECO DEVELOPMENT, INC.



1118 112TH AVE NE | SUITE 500  
BELLEVUE, WA | 98004  
1 425 483 2000 | 1 425 483 2002



1721 8th Ave N  
Seattle, WA 98109  
206 325 0877  
bergerpartnership.com

SITE PLAN REVIEW

04.25.2014



PAUL BROOKES  
DRAWN BY: MEYER

LANDSCAPE  
PLAN

L101

PLANT SCHEDULE

EXISTING TREES TO REMAIN

DET. BOTANICAL NAME

COMMON NAME

SIZE

WESTERN YARROW

TUFTED HARBASS

1 GAL @ 24" O.C.

1 GAL @ 24" O.C.

WESTERN YARROW

TUFTED HARBASS

1 GAL @ 24" O.C.

1 GAL @ 24" O.C.

NORTHWEST NATIVE WYCKLOVER AND GRASS MIX

ORNAMENTAL SHRUB PLANTING

HARDSCAPE SCHEDULE

PAVED COURTYARD

ROOFTOP TERRACE

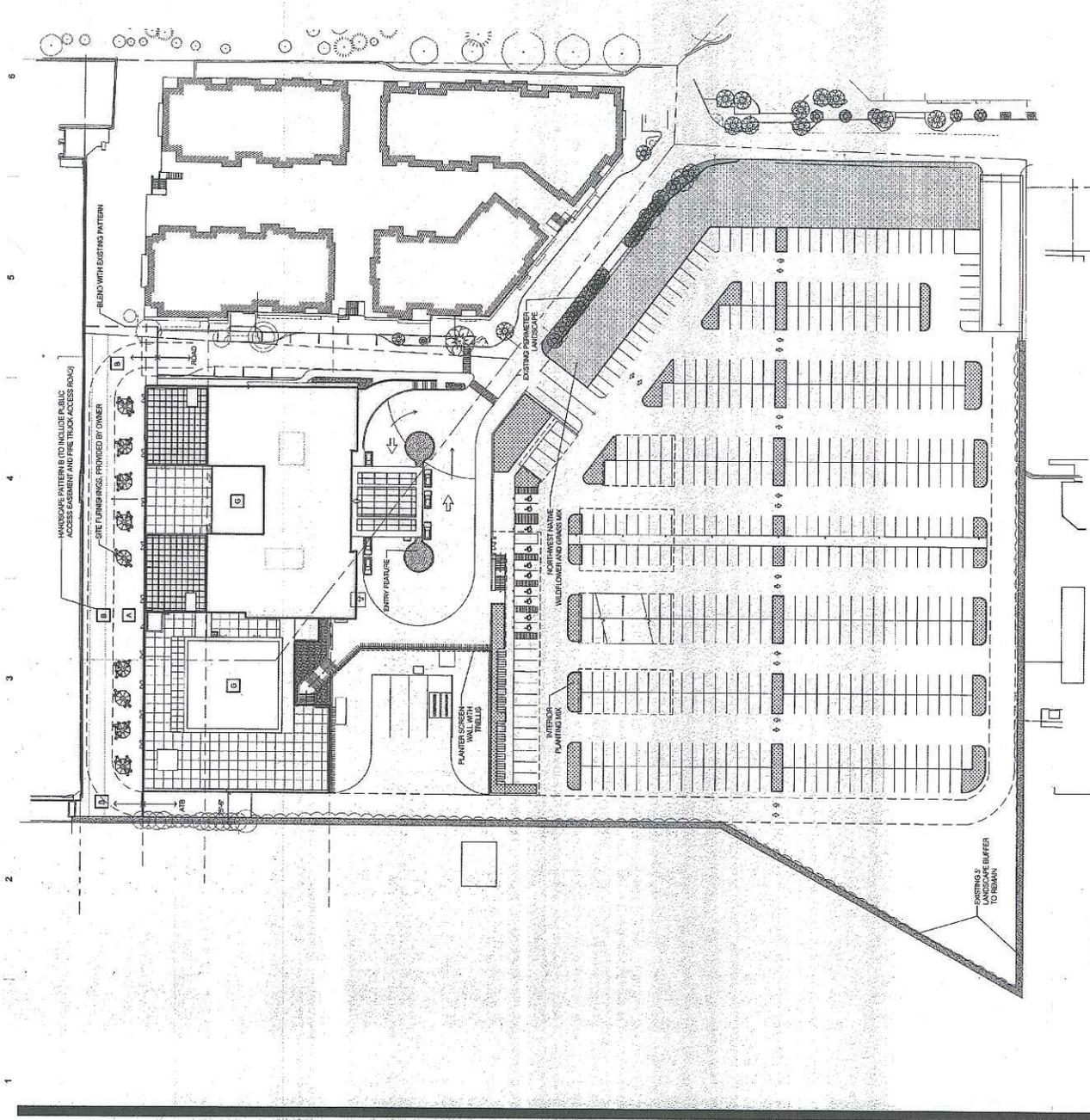
GREEN ROOF

HARDSCAPE PATTERN A

HARDSCAPE PATTERN B

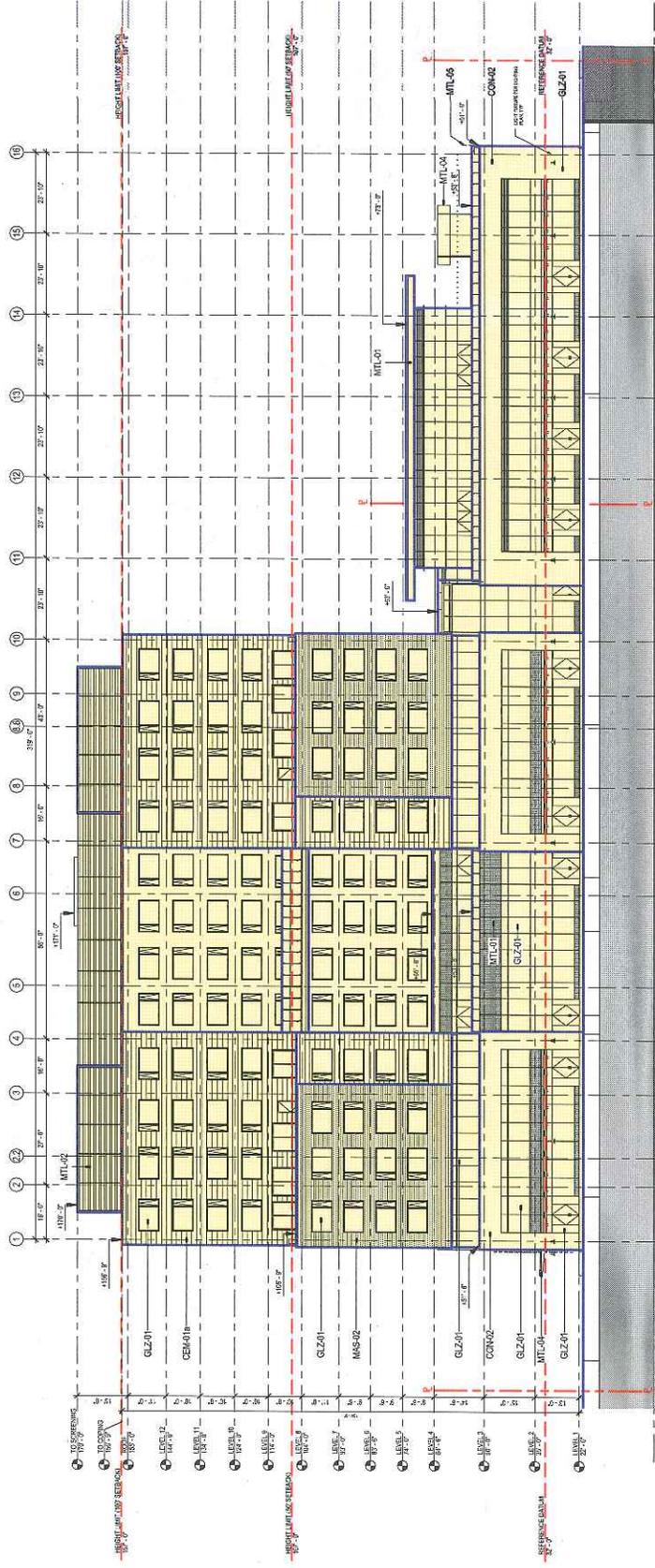
HARDSCAPE PATTERN C

ENTRY FEATURE



1 LANDSCAP SCALE 1" = 47'

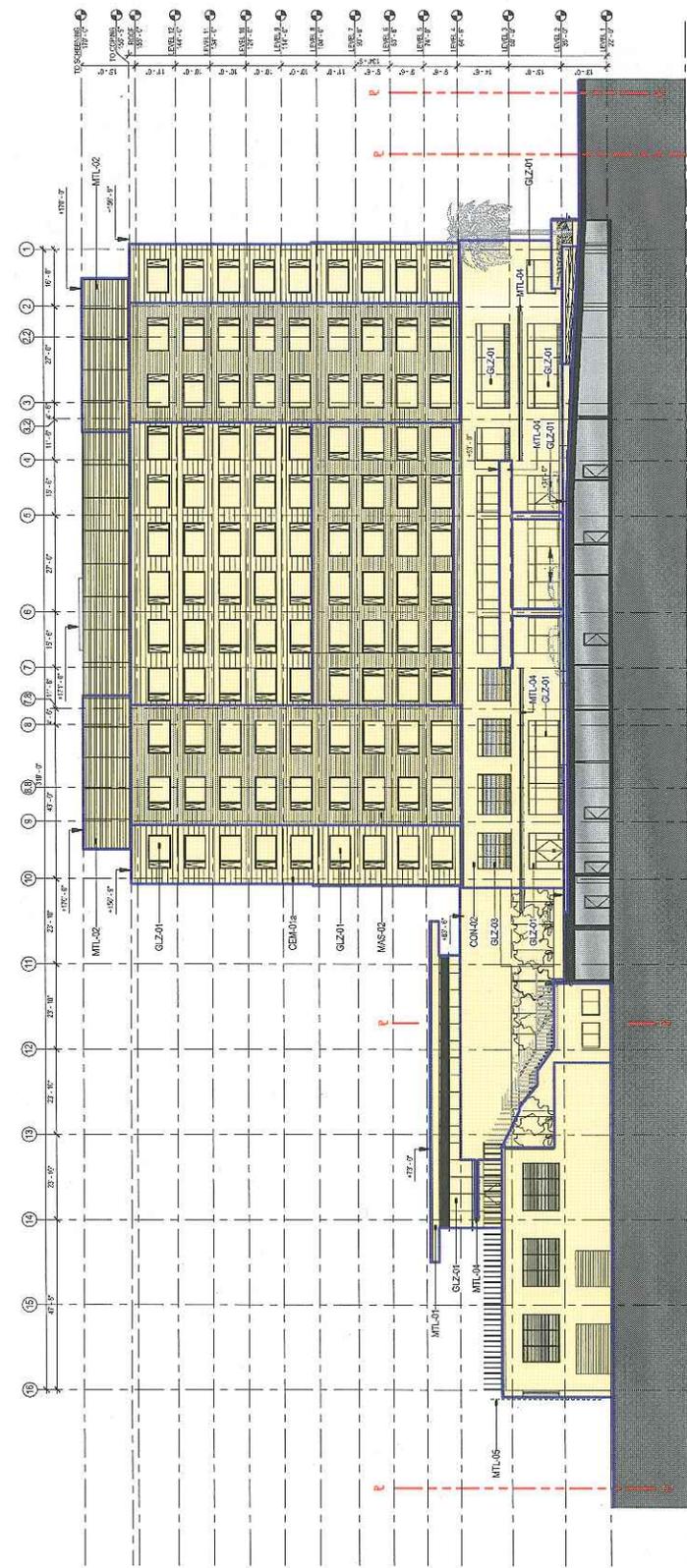
EXHIBIT 4



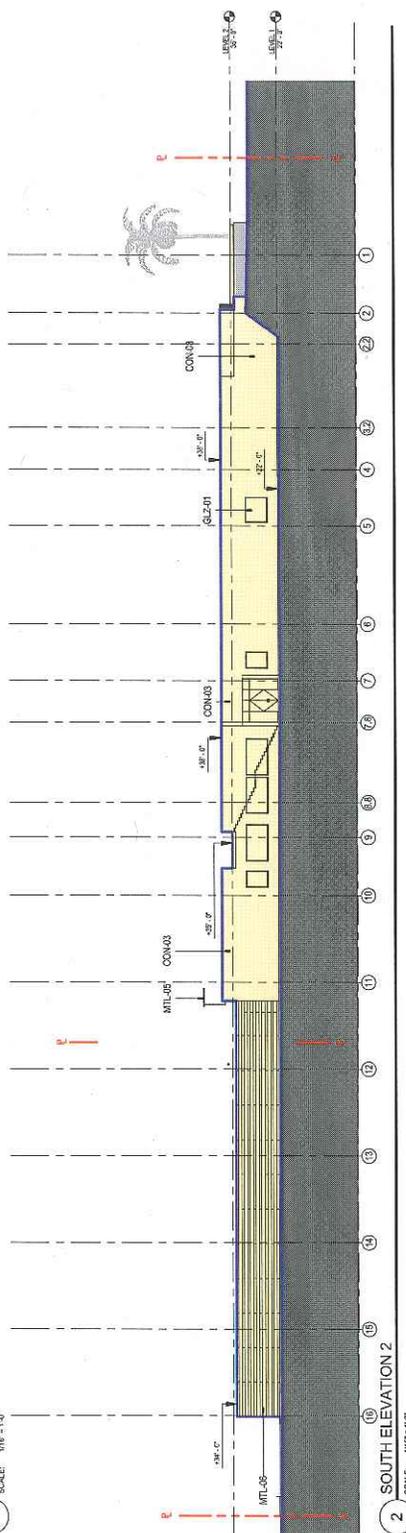
1 NORTH ELEVATION  
SCALE: 1/8" = 1'-0"

KEYNOTE	DESCRIPTION
CEL-01	CEMENTITIOUS PANEL A
CON-02	PRECAST CONC. PANEL
CON-03	CONCRETE
GLZ-01	INSULATED GLASS UNIT
GLZ-03	SCREEN IS UNIT
MAS-01	STONE
MAS-02	BRICK
MTL-01	METAL PANEL
MTL-02	PERFORATED
MTL-04	CANOPY
MTL-05	SCREEN
MTL-06	LOADING SCREEN
MTL-07	SIGNAGE
MTL-08	METAL DOOR

EXHIBIT 5

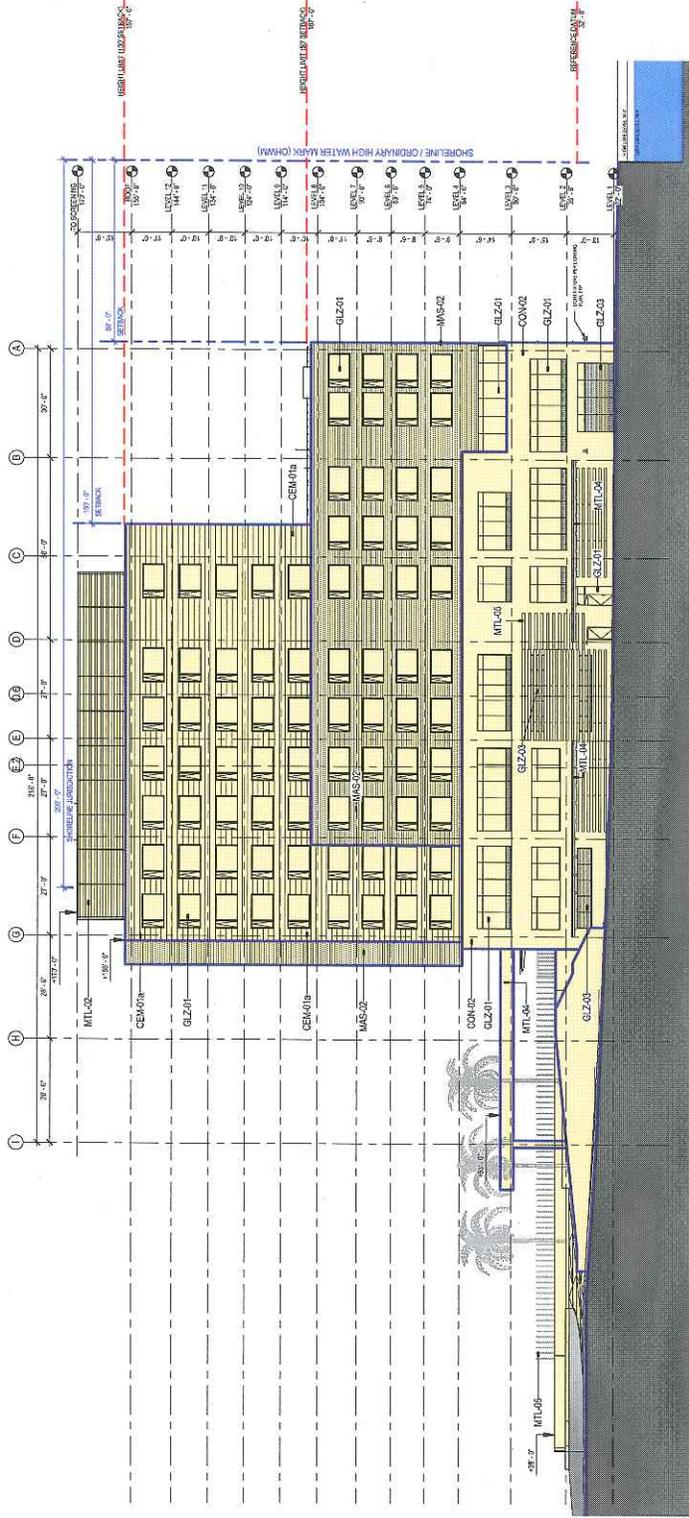


1 SOUTH ELEVATION  
SCALE: 1/8" = 1'-0"



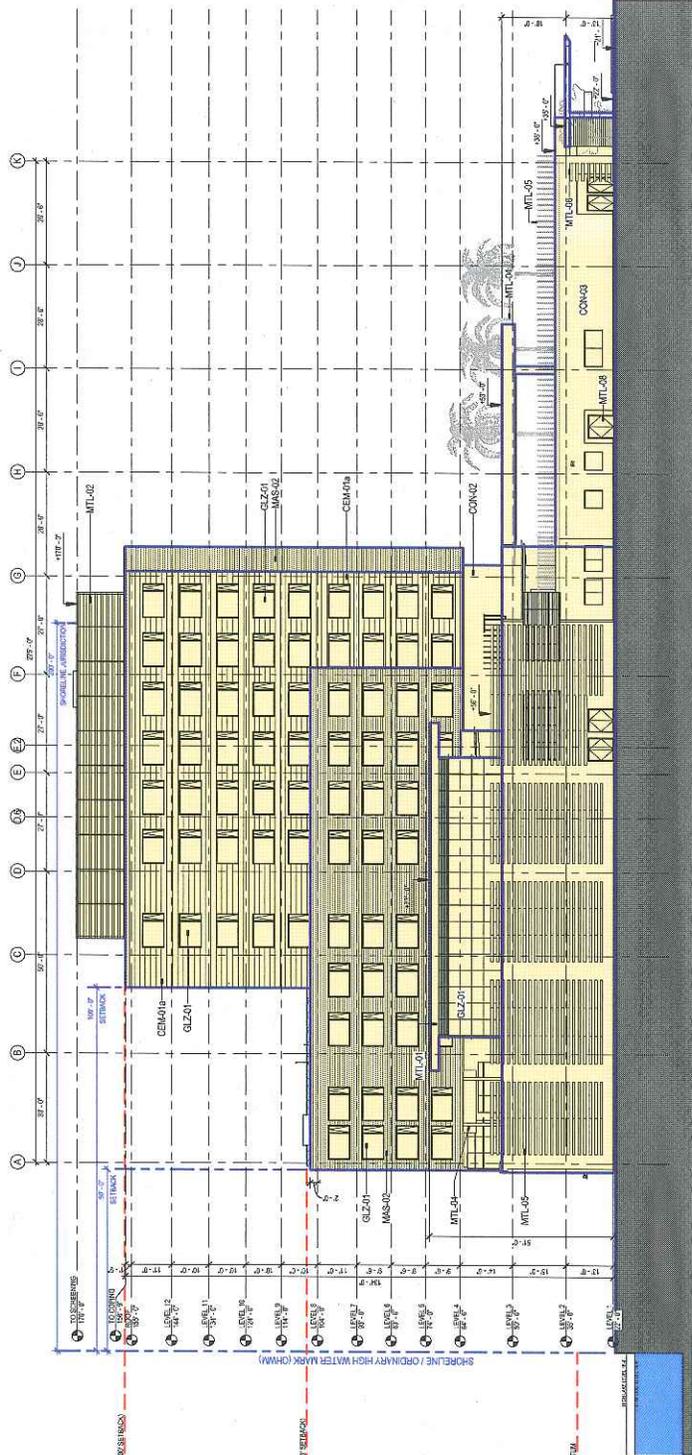
2 SOUTH ELEVATION 2  
SCALE: 1/8" = 1'-0"

KEYNOTE	MATERIAL KEYNOTE	DESCRIPTION
CER-01B	CERMENTITIOUS PANEL A	
CON-02	PRECAST CONCRETE PANEL	
GLZ-03	INSULATED GLASS UNIT	
GLZ-04	SCREENED GLASS UNIT	
MAS-01	STONE	
MAS-02	BRICK	
MTL-01	METAL PANEL	
MTL-02	CORRUGATED METAL PANEL	
MTL-03	SCREEN	
MTL-04	LOADING SCREEN	
MTL-07	SIGNAGE	
MTL-08	METAL DOOR	



1 EAST ELEVATION  
SCALE: 1/8" = 1'-0"

KEYNOTE	DESCRIPTION
CEM-01a	CEMENTITIOUS PANEL A
CON-02	PRECAST CONC. PANEL
GLZ-01	INSULATED GLASS UNIT
GLZ-03	SCREEN IG UNIT
MAS-01	STONE
MAS-02	BRICK
MTL-01	METAL PANEL
MTL-02	METAL CLAD
MTL-04	CANOPY
MTL-05	SCREEN
MTL-06	LOADING SCREEN
MTL-07	SIGNAGE
MTL-08	METAL DOOR



1 WEST ELEVATION  
SCALE: 1/8" = 1'-0"

KEYNOTE	DESCRIPTION
CEM-01a	CEMENTITIOUS PANEL A
CON-03	PRECAST CONC. PANEL
GLZ-01	INSULATED GLASS UNIT
MAS-01	STONE
MAS-02	BRICK
MTL-02	SCREEN
MTL-04	SCREEN
MTL-05	SCREEN
MTL-06	LOADING SCREEN
MTL-07	SIGNAGE
MTL-08	METAL DOOR

# Transportation Assessment

**DATE:** May 13, 2014

**TO:** Bill Stalzer  
Stalzer Associates

**FROM:** Jeff Schramm  
TENW

**SUBJECT:** Southport Transportation Assessment for Master Plan Minor Modification Request  
TENW Project No. 4838

**Entire Document  
Available Upon Request**

The purpose of this document is to provide a transportation assessment of the current Minor Modification Proposal to the approved Southport Master Plan. This document includes a trip generation analysis of the revised land uses in the master plan for the Southport Development and provides a comparison to the results of the PM peak hour trip generation analyses documented in the Master Plan Minor Modification of 2008 and the Level 2 Site Plan (LUA99-189, SA-A, SM) of July 18, 2000.

## Description of Master Plan Minor Modification

The current proposed Master Plan Minor Modification has the same land uses as those in the 2000 Level II Site Plan and 2008 Master Plan, but contains a slightly smaller hotel (350 rooms v. 355 rooms) than in the 2008 Master Plan, and larger hotel than the Level II Site Plan. The table below summarizes the land uses in all three Master Plans for the purposes of trip generation comparison.

Land Use	Type Units	7/18/2000 Approved Level II Site Plan	2/4/2008 Approved Master Plan	Proposed Master Plan Minor Modification
Residential	Units	395	383	383
Retail	SF	30,000	30,000	30,000
Restaurant	SF	10,000	10,000	10,000
Hotel	Rooms	220	355*	350
Office	SF	750,000	750,000	750,000
* Note: Up to 50 rooms could be for residential use.				

## Trip Generation Comparison

As stated previously, the Master Plan Minor Modification proposal has the same types of land uses as both previous Master Plans. Detailed PM peak hour trip generation calculations were developed for the the current Minor Modification Proposal, which are provided in the Attachment. For purposes of comparison, it should be noted that the same version of the Institute of Transportation Engineers (ITE) Trip Generation manual, 6<sup>th</sup> edition, was used to estimate trip generation for weekday PM peak hour conditions. Pass-by reductions for the

## ENVIRONMENTAL REVIEW COMMITTEE MEMO

APPLICATION NUMBER: LUA14-000645, SA-A, SM, MOD, MOD

APPLICANT: Hotel at Southport, LLC

PROJECT NAME: Southport Hotel

DESCRIPTION OF PROPOSAL: The applicant is requesting a Minor Master Site Plan Modification, Site Plan Review, Shoreline Substantial Development Permit, and a Parking Modification for the construction of a 325,500 square foot, 350 guest room hotel with associated meeting rooms, exercise facility, spa, restaurant, and support facilities. The property is located within the Urban Center North (UCN) land use designation and the Urban Center North - 2 (UCN-2) zoning classification. However, the proposal is vested to: COR-3 zoning classification as part of the Southport Development Planned Action FSEIS (September 9, 1999); the Southport Level II Master Plan and Shoreline Development Permit (LUA99-189, SA-A, SM); and subsequent Master Site Plan Modification (dated February 4, 2008). The approximate 11 acre site is currently vacant and contains a gravel parking lot. The hotel would be setback 50 feet from the Lake Washington ordinary high water mark (OHWM) and would have an approximate height of 75 feet within 100 feet of the OHWM and is 125 feet in height as it recedes from the shoreline. The applicant is proposing an interim surface parking lot containing 487 parking spaces on the Southport Office Building site (LUA00-156) until such time the approved offices are constructed. Permanent parking would be provided within shared use parking garages within the office buildings once constructed. The applicant is requesting a modification from RMC 4-4-080 in order to exceed the maximum number of parking stalls allowed by code. Access to the site is proposed from Lake Washington Blvd via an internal road network to the overall Southport Development site. The site is located within the Shoreline Jurisdiction.

A Final Supplemental Environmental Impact Statement (FSEIS) and mitigation document was issued for the Southport Planned Action in September 1999. Ordinance #4804 was adopted by the City Council in October 1999 designating a Planned Action of the subject site. The objective of the Southport Planned Action SEIS was to complete the environmental review upfront on the overall conceptual master site plan. Project applications and development phases are then allowed to proceed without additional environmental review provided the proposed development is consistent with the plan alternatives and level of development that was analyzed in the EIS process.

On April 4, 2000, the City determined that the Southport Master Site Plan was consistent with both the range of plan alternatives that were evaluated in the EIS and with the development levels or parameters as specified in the Planned Action Ordinance. The City approved a minor modification to the Master Site Plan on February 4, 2008. The City also determined that the modified Southport Master Site Plan (2008) was consistent with both the range of plan alternatives that were evaluated in the EIS and with the development levels or parameters as specified in the Planned Action Ordinance.

LOCATION OF PROPOSAL: 1083 Lake Washington Blvd

**EXHIBIT 7**

City of Renton  
Environmental Review Committee  
Department of Community & Economic Development

**DEPARTMENT OF COMMUNITY  
AND ECONOMIC DEVELOPMENT**



Upon determination by the City's Environmental Review Committee that the proposal meets the criteria outlined in the Planned Action Ordinance (Ordinance #4804) and qualifies as a planned action, the proposal shall not require a SEPA threshold determination, preparation of an EIS, or be subject to further review pursuant to SEPA.

The City's Environmental Review Committee has determined the proposed minor modification is consistent with the Master Site Plan which has been designated as a "planned action", pursuant to RCW 43.21C.030, as it meets all of the following conditions:

✓	The proposal is located within the Southport Planned Action Area.
✓	The proposed uses and activities are consistent with those described in the Planned Action EIS and Planned Action Qualifications.
✓	The proposal is within the Planned Action thresholds and other criteria of the Planned Action Qualifications.
✓	The proposal is consistent with the City of Renton Comprehensive Plan and applicable zoning regulations if all conditions of approval are met.
✓	The proposal's significant adverse environmental impacts have been identified in the Planned Action EIS.
✓	The proposal's impacts have been mitigated by application of the measures identified in Ordinance #4804, and other applicable City regulations, together with any modifications or variances or special permits that may be required.
✓	The proposal complies with all applicable local, state and/or federal laws and regulations, and the Environmental Review Committee determines that these constitute adequate mitigation.
✓	The proposal is not an essential public facility as defined by RCW.36.70A.200(1).

**SIGNATURES:**

\_\_\_\_\_  
Gregg Zimmerman, Administrator  
Public Works Department

\_\_\_\_\_  
Date

\_\_\_\_\_  
Mark Peterson, Administrator  
Fire & Emergency Services

\_\_\_\_\_  
Date

\_\_\_\_\_  
Terry Higashiyama, Administrator  
Community Services Department

\_\_\_\_\_  
Date

\_\_\_\_\_  
C.E. Vincent, Administrator  
Department of Community &  
Economic Development

\_\_\_\_\_  
Date

Entire Document  
Available Upon Request

**COUGHLINPORTERLUNDEEN**  
STRUCTURAL CIVIL SEISMIC ENGINEERING

/ DATE  
04/25/14

/ PROJECT  
Southport Hotel

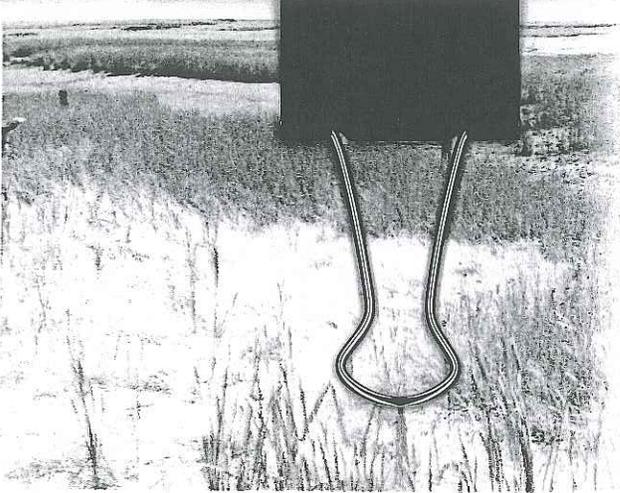
/ CLIENT  
Mulvanny G2  
Architecture

/ CONTENT  
Technical Information Report



**EXHIBIT 8**

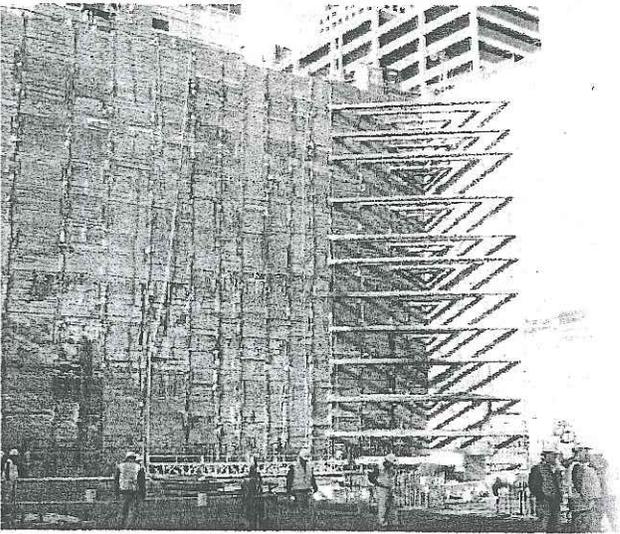
Entire Document  
Available Upon Request



Geotechnical Engineering Design Study

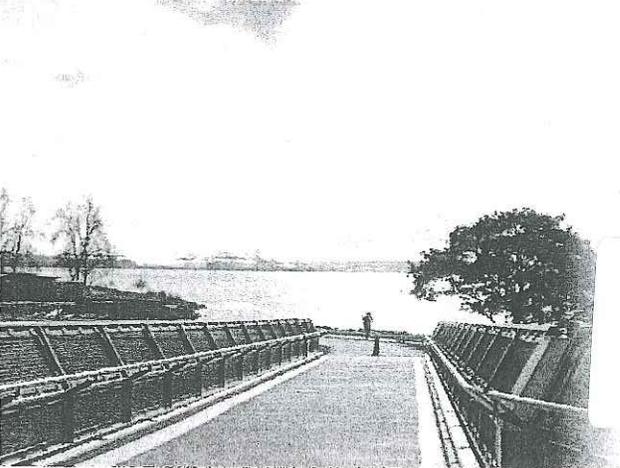
# Southport Hotel

Renton, Washington



Prepared for  
**Seco Development**

April 18, 2014  
19014-02



RECEIVED

MAY 19 2014

CITY OF RENTON  
PLANNING DIVISION

EXHIBIT 9

  
**HARTCROWSER**

# Aerial Photo



## Notes

None



256 0 128 256 Feet

WGS\_1984\_Web\_Mercator\_Auxiliary\_Sphere

## Legend

- Addresses
- Parcels

# EXHIBIT 10

**RDS FOR COMMERCIAL ZONING DESIGNATIONS \***

	CD	CO	COR
<b>LOT DIMENSIONS</b>			
Minimum Lot Size	None	25,000 sq. ft.	None
Minimum Lot Width	None	None	None
Minimum Lot Depth	None	None	None
<b>LOT COVERAGE</b>			
Maximum Lot Coverage for Buildings	None for properties located within the "Downtown Core Area"; <sup>15</sup> For properties located outside the Downtown Core Area: 65% of total lot area or 75% if parking is provided within the building or within a parking garage.	65% <sup>29</sup> of total lot area or 75% <sup>29</sup> if parking is provided within the building or within a parking garage.	65% of total lot area or 75% if parking is provided within the building or within a parking garage.
<b>DENSITY (Net Density in Dwelling Units per Acre)</b>			
Minimum Net Residential Density	25 dwelling units per net acre. The minimum density requirements shall not apply to the subdivision, short plat and/or development of a legal lot 1/2 acre or less in size as of March 1, 1995.	NA	Where a development involves a mix of uses then minimum residential density shall be 16 dwelling units per net acre. When proposed development does not involve a mix of uses, then minimum residential density shall be 5 dwelling units per net acre. The same area used for commercial and office development can also be used to calculate residential density. Where commercial and/or office areas are utilized in the calculation of density, the City may require restrictive covenants to ensure the maximum density is not exceeded should the property be subdivided or in another manner made available for separate lease or conveyance.

Conflicts: See RMC 4-1-080.

**Entire Document  
Available Upon Request**

4-3-060 (Reserved)

4-3-070 (Reserved)

**4-3-080 PUBLIC USE/"P" SUFFIX  
NOTIFICATION PROCEDURES:**

**A. NOTICE REQUIRED:**

The owner of any property designated with a "P" suffix shall be required to give written notice to the owners of all property within a three hundred foot (300') radius of the site involved, as well as all residents and/or businesses within a three hundred foot (300') radius of the site or facility, at least sixty (60) days in advance of any of the following:

1. A proposed change of use of the premises;
2. A proposed change of the major tenant and/or tenant group using the premises if such a change is determined by the Planning/Building/Public Works Administrator or designee to have probable major adverse impacts to the immediate surrounding area; or
3. Any proposed change of ownership of the premises.

Such notice shall not be required if the proposed change has been identified in a Master Plan adopted pursuant to the site development plan review regulations in chapter 4-9 RMC. The notice shall also invite these neighborhood property owners, residents and/or business persons to attend an informational meeting in the area, hosted by the owner of the property or their representative. (Ord. 5028, 11-24-2003)

**B. NOTICE CONTENT:**

The notices shall indicate that a summary of the meeting shall be prepared by the owner or owner's agent at least thirty (30) days in advance of any of the above three (3) actions in subsection A of this Section, Notice Required. At least fourteen (14) days in advance of the information meeting, the owner, agency, or organization hosting the meeting shall give general notice of the meeting in a local newspaper having broad circulation in the area. This meeting is intended to explain the proposed changes and invite citizen input

**SUMMARY:**

A summary of the meeting shall be mailed within seven (7) days of the meeting to the Development Services Division and to all in attendance who request, in writing, to receive the summary as well as parties that do not attend the meeting but request in writing to receive the summary. In addition, the Development Services Division shall receive and keep the summary of the meeting in its preapplication files for future reference.

**D. SPECIAL HEIGHT ALLOWANCES  
FOR PUBLICLY OWNED STRUCTURES:**

Any publicly owned structures on property designated with a "P" suffix shall be allowed to develop pursuant to the special height exceptions for such uses contained in RMC 4-2-110D, Condition 9, 4-2-110H, Condition 21, 4-2-120C, Condition 20, or 4-2-130B, Condition 13, as well as under the Master Plan review regulations. (Ord. 4523, 6-5-1995; Amd. Ord. 4963, 5-13-2002; Ord. 5028, 11-24-2003)

**4-3-090 SHORELINE MASTER  
PROGRAM REGULATIONS:**

**A. PROGRAM ADOPTED:**

The Shoreline Master Program, as issued and prepared by City of Renton Planning Commission, of which one printed copy in book form has heretofore been filed and is now on file in the office of the City Clerk and made available for examination by the general public, is hereby adopted as the Shoreline Master Program by the City of Renton. (Ord. 3758, 12-5-1983, Rev. 7-22-1985 (Min.), 3-12-1990 (Res. 2787), 7-16-1990 (Res. 2805), Rev. 9-12-1993 (Min.), Ord. 4716, 4-13-1998)

The following is an excerpt from the officially adopted Shoreline Master Program. The complete Shoreline Master Program, including policies, should also be consulted. (Ord. 4722, 5-11-1998)

**B. AUTHENTICATION, RECORD OF  
PROGRAM:**

The City Clerk is hereby authorized and directed to duly authenticate and record a copy of the above mentioned Shoreline Master Program together with any amendments or additions thereto, together with an authenticated copy of this Section. (Ord. 3094, 1-10-1977, eff. 1-19-1977)

CITY OF RENTON, WASHINGTON

Entire Document  
Available Upon Request

ORDINANCE NO. 4804

**AN ORDINANCE OF THE CITY OF RENTON, WASHINGTON DESIGNATING A PLANNED ACTION FOR THE SOUTHPORT SITE, APPROXIMATELY 17 ACRES LOCATED ADJACENT TO LAKE WASHINGTON AND BETWEEN GENE COULON PARK ON THE EAST AND BOEING MANUFACTURING OPERATIONS ON THE WEST.**

WHEREAS, RCW 43.21C.031 and WAC 197-11-164, - 168, and -172 allow and govern the application of a Planned Action designation; and,

WHEREAS, in Resolution No. 3379, the City indicated its intent to study and consider a Planned Action designation for the Shuffleton Steam Plant site, also known as the Southport site; and,

WHEREAS, a Supplemental Environmental Impact Statement (SEIS) has been prepared for a portion of the Shuffleton Steam Plant site, entitled the *Southport Development Planned Action Supplemental Environmental Impact Statement*, and such document considers the potential environmental impacts of a phased mixed-use project on approximately 17 acres of property within the City limits; and,

WHEREAS, with Ordinance No. 4796, the City has amended the Comprehensive Plan Land Use Map for the subject area from Employment Area – Industrial (EA-I) to Center Office Residential (COR); and,

WHEREAS, with Ordinance No. 4800, the City has amended the Zoning Map from Heavy Industrial (IH) to Center Office Residential (COR); and,

WHEREAS, this Ordinance would designate certain land uses and activities as “Planned Actions” which would be consistent with the Center Office Residential (COR) designation and zone;

**NOW, THEREFORE, THE CITY OF RENTON DOES ORDAIN:**

**SECTION I. Purpose.** The City of Renton declares that the purpose of this ordinance is to:

A. Set forth a procedure designating certain project actions within the subject site as “Planned Actions” consistent with state law, RCW 43.21C.031; and,

B. Provide the public with an understanding as to what constitutes a Planned Action and how land use applications which qualify as Planned Actions will be processed by the City; and,

C. Streamline and expedite the land use permit review process for this site by relying on completed and existing detailed environmental analysis for the subject site; and,

## A. Level II Site Plan Review

<b>DECISION DATE:</b>	July 18, 2000
<b>Project Name:</b>	Southport Level II Site Plan
<b>Applicant:</b>	Michael Christ SECO Development Southport, LLC 10843 NE 8 <sup>th</sup> Street, Suite 200 Bellevue, WA 98004
<b>Owner:</b>	Same as applicant
<b>Contact Person:</b>	Rex Allen SECO Development 10843 NE 8 <sup>th</sup> Street, Suite 200 Bellevue, WA 98004
<b>File Number:</b>	LUA-99-189, SA-A, SM
<b>Project Manager:</b>	Elizabeth Higgins
<p><b>Project Description (revised):</b> The applicant, SECO Development, requests Level II Site Plan Review. The Level II Site Plan for Southport proposes a mixed-use development consisting of six buildings. The development would include a 7-story, 220 room hotel above one level of parking (220 parking stalls), two residential buildings with 4-stories of residential units above two levels of parking (682 parking stalls) that include a total of 395 residential units, and three office buildings with 7-stories of offices above four levels of parking (1,855 parking stalls). The office buildings would include a total approximately 720,000 to 750,000 square feet. These development amounts are consistent with the Southport Planned Action Ordinance, Mitigation Document, and Final Supplemental Environmental Impact Statement</p> <p>The proposal includes a promenade along the waterfront of Lake Washington. Public access to the promenade would be provided from Gene Coulon Park, a City park adjacent to the north of the subject site. Off-site improvements would consist of improvements to Lake Washington Boulevard between Park Drive and the project entrance, signal and channelization modifications at the Park Drive/Lake Washington Boulevard intersection, and signalization of the project entrance/Lake Washington Boulevard intersection.</p> <p>On April 5, 2000, the Southport Level II Site Plan was deemed to be a Planned Action. On April 7, 2000, the City of Renton issued a Shoreline Substantial Permit for the Southport Level II Site Plan. These actions were not appealed before the proscribed appeal periods terminated.</p> <p>Following approval of the Southport Level II Site Plan, a more detailed, Level I Site Plan Review would be required for subsequent, individual, phases of the proposed master site plan. The Level I Site Plans must be consistent with the overall Level II site plan as approved. Building permits may not be issued until approval of the Level I Site Plans.</p>	
<b>Project Location:</b>	1101 Lake Washington Boulevard North
<b>Project Location:</b>	1101 Lake Washington Boulevard North
<b>Site Area:</b>	Approximately 17 acres



Denis Law, Mayor

# CITY OF RENTON

Planning/Building/Public Works Department  
Gregg Zimmerman P.E., Administrator

February 4, 2008

Bill Stalzer  
Stalzer and Associates  
603 Stewart Street, Suite 419  
Seattle, WA 98101

**Entire Document  
Available Upon Request**

**SUBJECT: MINOR MODIFICATION REQUEST FOR SOUTHPORT MASTER PLAN (FILE NO. LUA-99-189, SA-A SM)**

Dear Mr. Stalzer:

I am in receipt of your letter and attachments of January 23, 2008, wherein you request approval of a minor modification to the approved master plan for the Southport development. The request is summarized below.

### Request

1. The office square footage is reduced from 780,000 square feet to 750,000 square feet, consistent with the Level I Office Site Plan approved on February 5, 2001.
2. The number of hotel rooms is increased from 220 rooms to 355 rooms, 55 of which could be residential units. This change increases the size of the hotel area devoted to guest rooms and lobbies to 212,502 square feet.
3. The retail area is reduced in size from 33,000 square feet to 30,000 square feet.
4. The restaurant size is increase in size from 7,000 square feet to 10,000 square feet.

### Site Plan Modification Criteria

Renton Municipal Code, Section 4-9-200I, allows minor adjustments to an approved master plan, provided:

1. *The adjustment does not involve more than a ten percent (10%) increase in area or scale of the development in the approved site plan; or*
2. *The adjustment does not have a significantly greater impact on the environmental and facilities than the approved plan; or*
3. *The adjustment does not change the boundaries of the originally approved plan.*

### Analysis of Request

The master plan modifications requested and as shown in your January 23, 2008, submittals have been compared to the master plan (Level II site plan) approved on July 18, 2000. Based on staff's analysis, I have determined the proposed revisions Nos. 1 through 4 (as described above)

**EXHIBIT 15**

Idy Way - Renton, Washington 98057



This paper contains 50% recycled material, 30% post consumer

**RENTON**  
AHEAD OF THE CURVE



Attachment A

Entire Document  
Available Upon Request

DESIGNATION OF A PROJECT APPLICATION AS A PLANNED

<b>DECISION DATE:</b>	April 4, 2000
<b>Project Name:</b>	Southport Level 2 Site Plan
<b>Applicant:</b>	Michael Christ SECO Development Southport, LLC 10843 NE 8 <sup>th</sup> Street, Suite 200 Bellevue, WA 98004
<b>Owner:</b>	Same as applicant
<b>Contact Person:</b>	Rex Allen SECO Development 10843 NE 8 <sup>th</sup> Street, Suite 200 Bellevue, WA 98004
<b>File Number:</b>	LUA-99-189, SA-A, SM
<b>Project Manager:</b>	Peter Rosen
<b>Project Description:</b>	<p>The applicant, SECO Development, requests approval of a Level II site plan and a Shoreline Substantial Development Permit for the Southport Development. The Level II master site plan for Southport proposes a mixed-use development consisting of six buildings. The development would include a 7-story, 220 room hotel above one level of parking (220 parking stalls), two residential buildings with 4-stories of residential units above two levels of parking (682 parking stalls) that includes a total of 395 residential units, and three office buildings with 7-stories of offices above four levels of parking (1,855 parking stalls). The office buildings would include a total approximately <del>720,200</del> <sup>750,000</sup> square feet.</p> <p>The proposal includes a promenade along the waterfront of Lake Washington. Public access to the promenade would be provided from Gene Coulon Park, a City park adjacent to the north of the subject site. Off-site improvements would consist mainly of improvements to Lake Washington Boulevard between Park Drive and the project entrance, signal and channelization modifications at the Park Drive/Lake Washington Boulevard intersection, and signalization of the project entrance/Lake Washington Boulevard intersection.</p> <p>A more detailed, Level I site plan review will be required for the individual phases of the proposed master site plan. The phases must be consistent with the overall Level II site plan. Building permits may not be issued until approval of the Level I site plan.</p>
<b>Project Location:</b>	1101 Lake Washington Boulevard North
<b>Site Area:</b>	Approximately 17 acres

Planned Action Review Criteria.

1. *The Director of Development Services, or the Director's designee, is authorized to designate a project application as a Planned Action pursuant to RCW 43.21C.031(2)(a), if the project application meets all of the following conditions:*
  - a) *The project is located on the subject site as described in Section III.A., or is an off-site improvement directly related to a proposed development on the subject site; and,*

Entire Document  
Available Upon Request

---

---

**SOUTHPORT PLANNED ACTION  
MITIGATION DOCUMENT**



**Prepared By:**

**City of Renton  
Economic Development/Neighborhoods & Strategic Planning Department**

**With Assistance By:  
Bucher, Willis and Ratliff Corporation**

**September 17, 1999**

---

---

**EXHIBIT 17**

# MEMORANDUM

**DATE:** April 28, 2014  
**TO:** Bill Stalzer, Stalzer and Associates  
**FROM:** Jeff Schramm  
 TENW  
**SUBJECT:** Parking Analysis for Southport Hotel  
 TENW Project No. 4838

This memorandum documents the parking demand and proposed supply for the Southport Hotel in accordance with the requirements of RMC 4-4-080.F.10 "Number of Parking Spaces Required" for Hotels, Motels and Cabins in effect at the time of approval of the Southport development and consistent with the parking analysis methodology employed in the Southport Development Planned Action Final Supplemental EIS for a hotel. The parking analysis for a hotel is contained in Table 3 "Plan C Parking Summary" on page 2-5 of the Final SEIS.

RMC 4-4-080.F.10 requires 1 space for each guest room or dwelling unit plus 2 for each 3 employees. The proposed hotel contains 350 guest rooms, resulting in a code requirement for 350 parking spaces. The hotel would have 175 employees<sup>1</sup> resulting in a code requirement for 117 spaces for hotel employees.

Use	Number	Code Requirement
Hotel Rooms	350	350
Employees	175	117
Total		467

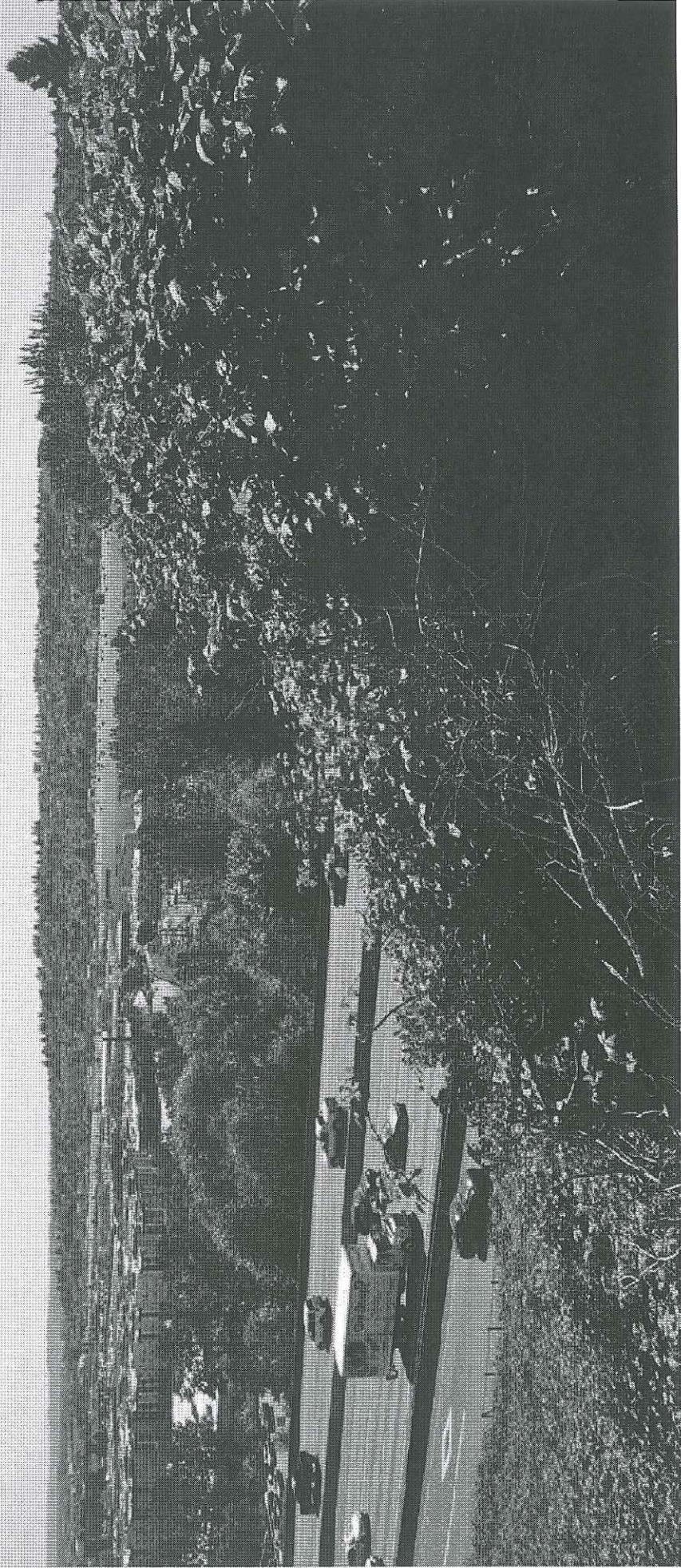
The estimated peak parking demand for guests and employees for a 350 room hotel is 315 parking spaces based on the ITE 2<sup>nd</sup> Edition of the Parking Generation manual.<sup>2</sup> This is the proposed number of parking spaces for the hotel. Since the number of spaces provided in the parking lot differs from the city code requirement, a modification request meeting the criteria in the city code is being proposed.

Please contact me if you have questions or need additional information. You can reach me at [schramm@tenw.com](mailto:schramm@tenw.com) or 425-250-0581.

<sup>1</sup> Footnote 2 to Table 3 "Plan C Parking Summary" page 2-5 of Southport Development Planned Action Final Supplemental EIS.

<sup>2</sup> Footnote 1 to Table 3 "Plan C Parking Summary" page 2-5 of Southport Development Planned Action Final Supplemental EIS.

**EXHIBIT 19**

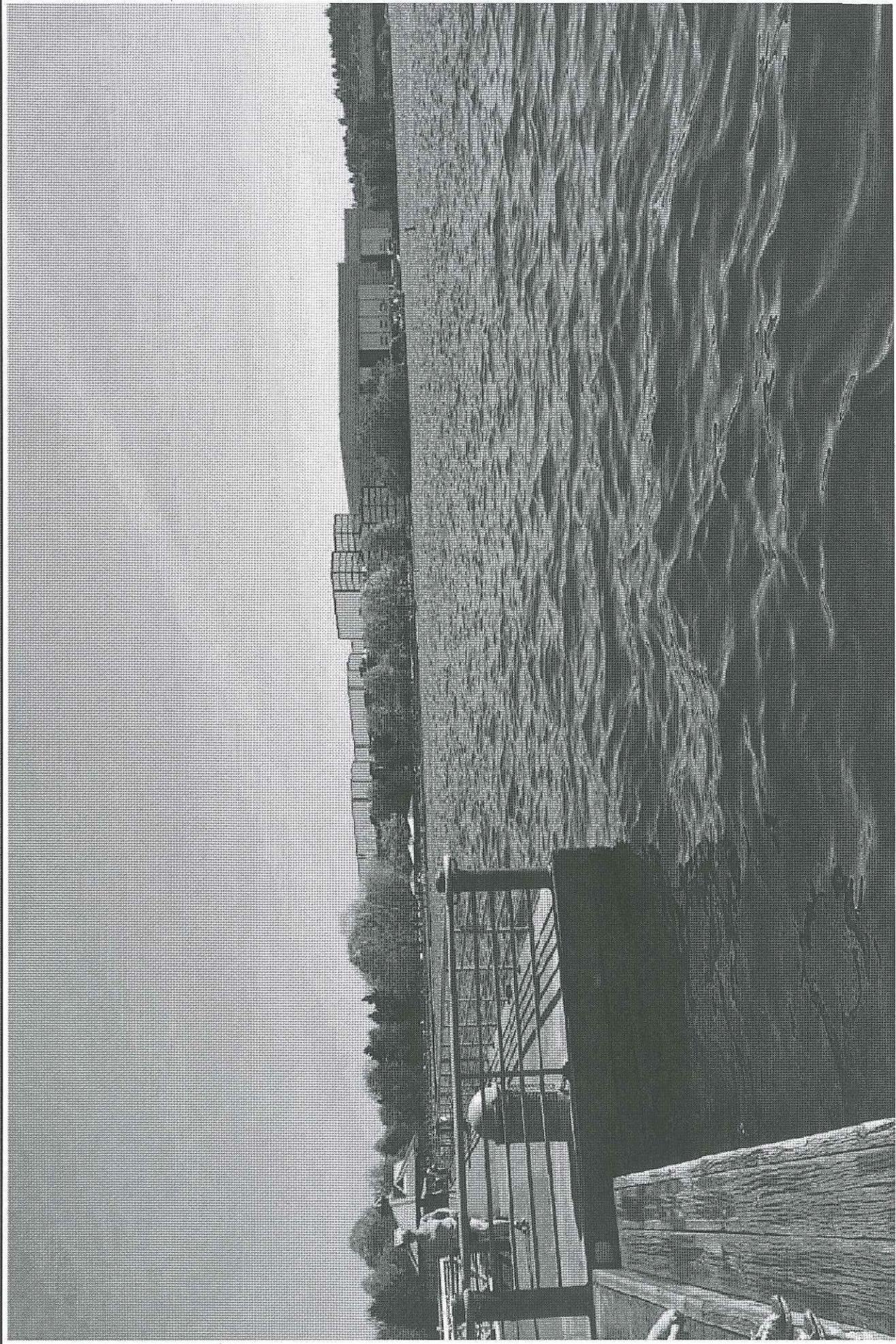


Southport Master Plan  
Minor Modification

May, 2014

Hotel at Southport, LLC

Figure 2: View from Jones Ave. N.E.  
at 14th Street

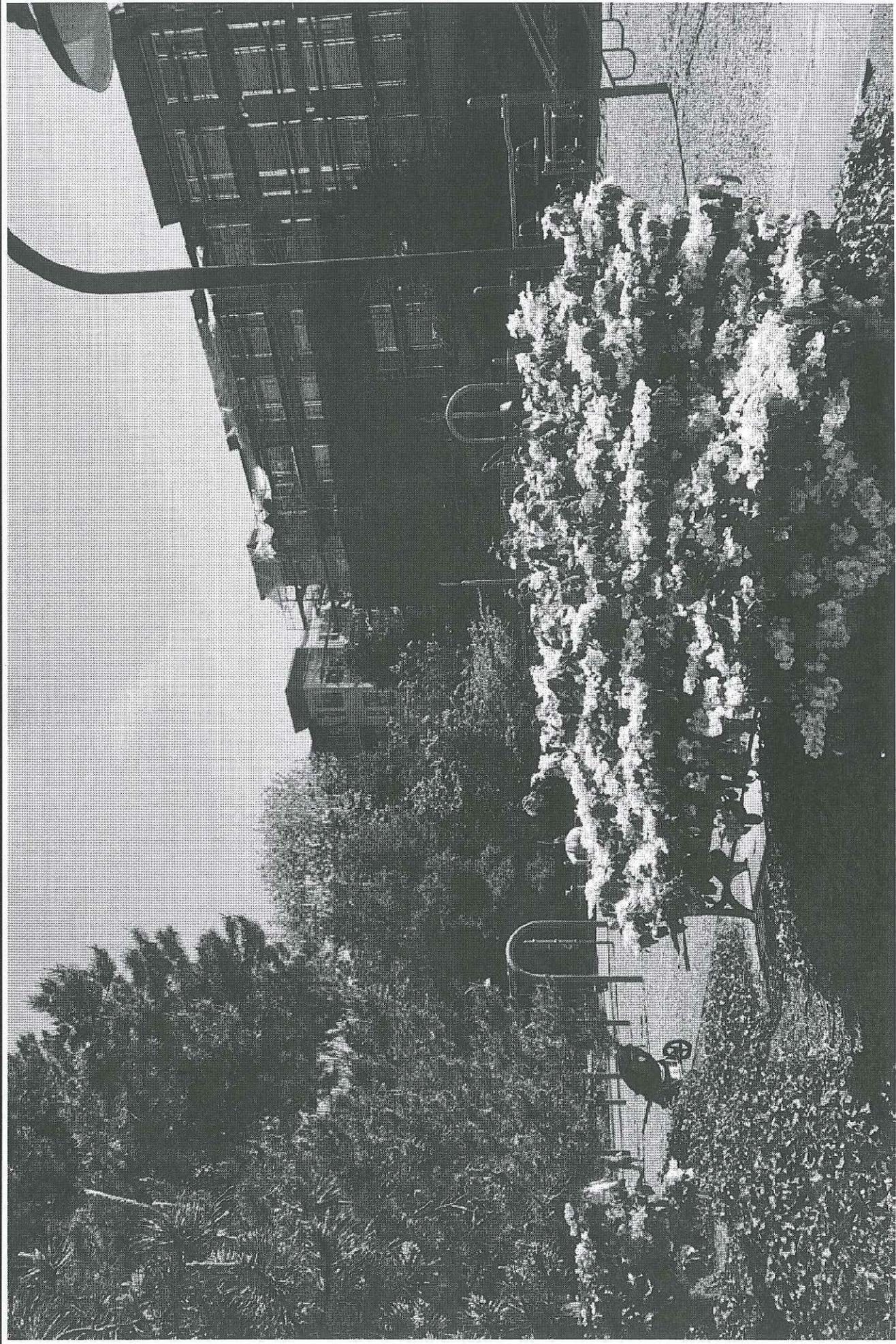


Southport Master Plan  
Minor Modification

May, 2014

Hotel at Southport, LLC

Figure 3: View from Gene Coulon  
Park Docks

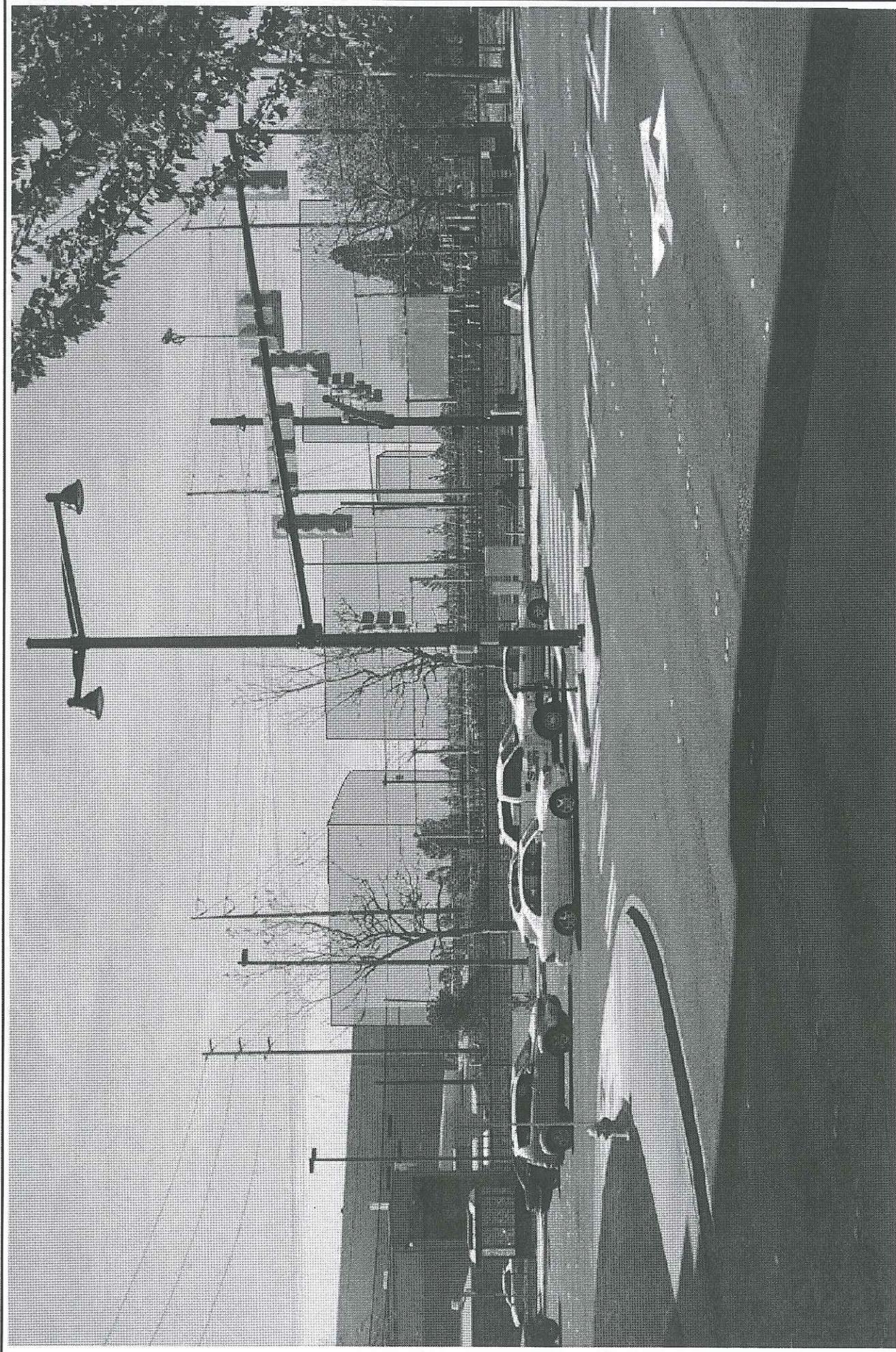


Southport Master Plan  
Minor Modification

May, 2014

Hotel at Southport, LLC

Figure 4: View from Gene Coulon  
Park Playground



Southport Master Plan  
Minor Modification

May, 2014

Hotel at Southport, LLC

Figure 5: View from Park Ave



1 2 3 4 5 6 7 8 9

THE HOTEL AT SOUTHPORT



RENTON, WASHINGTON



SECO DEVELOPMENT, INC.



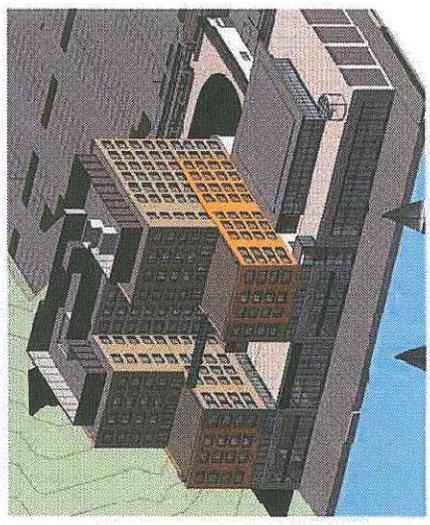
119127HARV. NE | SUITE 600  
 BELLEVUE, WA | 98004  
 425-483-2000 | 425-483-2000  
 MulvannyGroup.com

UPPER STORY SETBACKS MODIFICATION REQUEST

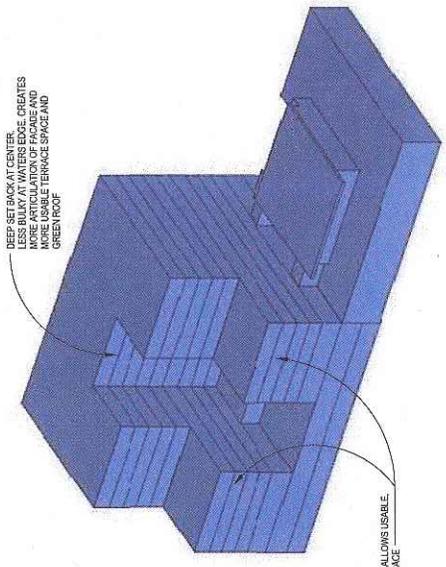
05.16.2014

PR: DAVENPORT  
 DRAWN BY: WHITE

UPPER STORY SETBACK MASS DIAGRAMS  
 A950



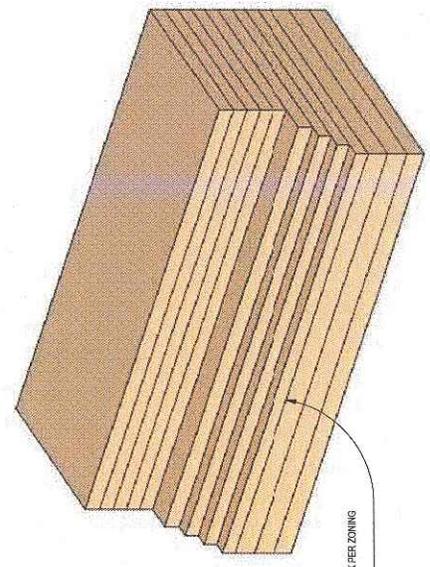
1 PROPOSED HOTEL RENDERING



DEEP SET BACK AT CENTER  
 LESS BULKY AT WATERS EDGE. CREATES  
 MORE ARTICULATION OF FACADE AND  
 AVAILABLE TERRACE SPACE AND  
 GREEN ROOF.

NO STEP BACK AT ENDS ALLOWS USABLE,  
 ENHANCED TERRACE SPACE.

2 PROPOSED HOTEL BUILDING MASS



BUILDING MASS STEPS BACK PER ZONING  
 CODE  
 RMC 4-2-100B

3 ALLOWABLE BUILDING MASS PER RMC 4-2-120B UPPER  
 STORY SETBACK BUILDING MASS

RENTON MUNICIPAL CODE  
 ZONE: COR 3  
 RMC 4-2-120B DEVELOPMENT STANDARDS  
 UPPER STORY SETBACKS  
 APPROVAL REQUESTED THROUGH  
 MODIFICATION PROCESS  
 PURSUANT TO RMC 4-9-250D

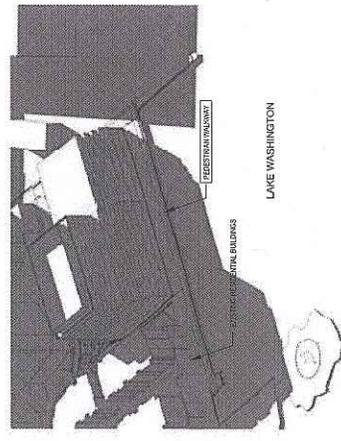
EXHIBIT 21

CITY OF RENTON  
SHORELINE MANAGEMENT ACT OF 1971  
PERMIT FOR SHORELINE MANAGEMENT  
SHORELINE SUBSTANTIAL DEVELOPMENT PERMIT

APPLICATION NO.: LUA99-189, SA-A, SM  
DATE RECEIVED: December 27, 1999  
DATE OF PUBLIC NOTICE: February 17, 2000  
DATE APPROVED: April 7, 2000  
DATE DENIED: NA  
TYPE OF ACTION(S):  Substantial Development Permit  
 Conditional Use Permit  
 Variance Permit  
APPLICANT: SECO Development  
PROJECT: Southport Level 2 Site Plan  
LEGAL DESCRIPTION: Attached separately to report.  
SEC-TWNP-R: Section 8, Township 23 North, Range 5 East, W.M.  
WITHIN SHORELINES OF: Lake Washington  
APPLICABLE MASTER PROGRAM: City of Renton

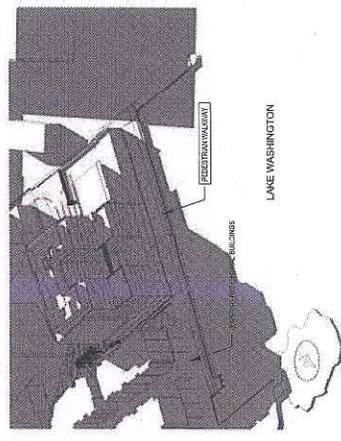
1 2 3 4 5 6 7 8

HOTEL PER RMC 4-2-120B UPPER STORY SETBACK



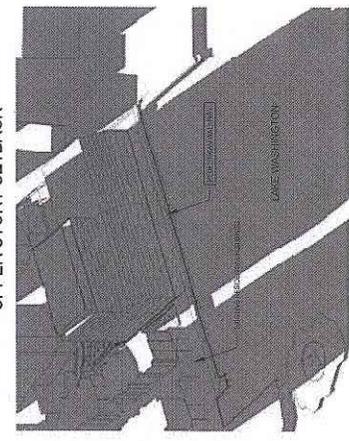
UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
A1 DECEMBER 21 - NOON - WINTER SOLSTICE

PROPOSED HOTEL



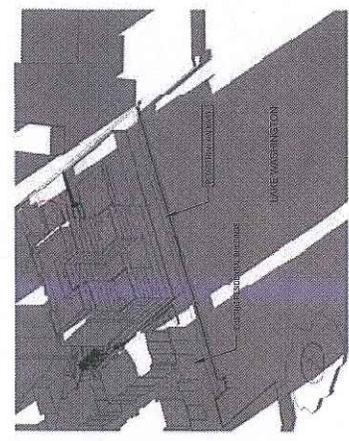
UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
B1 DECEMBER 21 - NOON - WINTER SOLSTICE

HOTEL PER RMC 4-2-120B UPPER STORY SETBACK

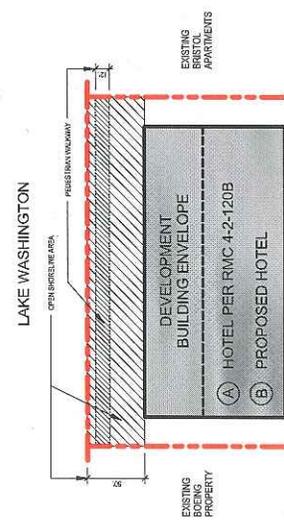


UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
A2 DECEMBER 21 - 9 AM - WINTER SOLSTICE

PROPOSED HOTEL



UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
B2 DECEMBER 21 - 9 AM - WINTER SOLSTICE



**SOLAR ACCESS STUDY - OBJECTIVE**  
THIS SOLAR ACCESS STUDY DEMONSTRATES AND COMPARES SOLAR DEVELOPMENT CONDITIONS CREATED BY DEVELOPMENT (A) AND DEVELOPMENT (B) OVER THE OPEN SHORELINE AND PUBLIC ACCESS EASEMENT AS SHOWN.

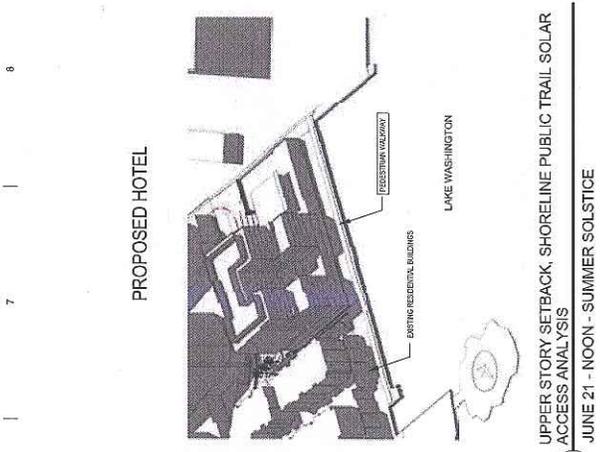
SOLAR ACCESS STUDY - DETAIL KEY TABULATION		DEVELOPMENT ENVELOPE TO COMPARE	
DAYS WITH SOLAR ACCESS		A - PER RMC 4-2-120B	
		B - PROPOSED	
WINTER SOLSTICE	NOON	(A)	(B)
DECEMBER 21	9AM	(A)	(B)
SPRING FALL EQUINOX	NOON	(A)	(B)
MARCH 20 & SEPT 23	9AM	(A)	(B)
SUMMER SOLSTICE	NOON	(A)	(B)
JUNE 21	9AM	(A)	(B)

■ DEVELOPMENT WITH MAXIMUM ALLOWABLE BUILDING ENVELOPE (UPPER STORY SETBACK PER RMC 4-2-120B)  
■ DEVELOPMENT WITH PROPOSED ALTERNATIVE BUILDING ENVELOPE (UPPER STORY SETBACK)

**LEGEND**  
 AREA WITH SOLAR ACCESS  
 AREA IN SHADE (NO SOLAR ACCESS)

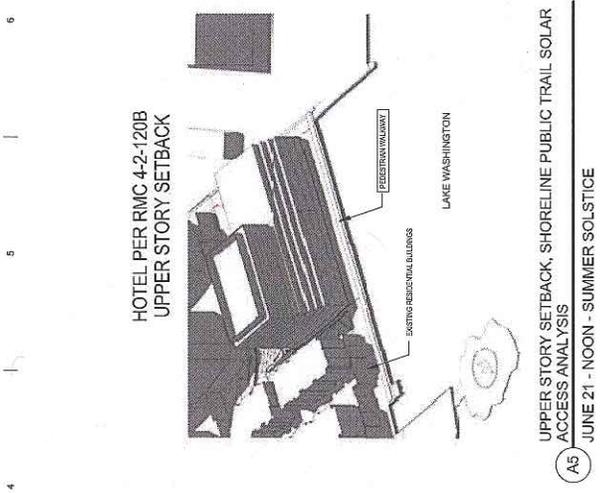
EXHIBIT 23





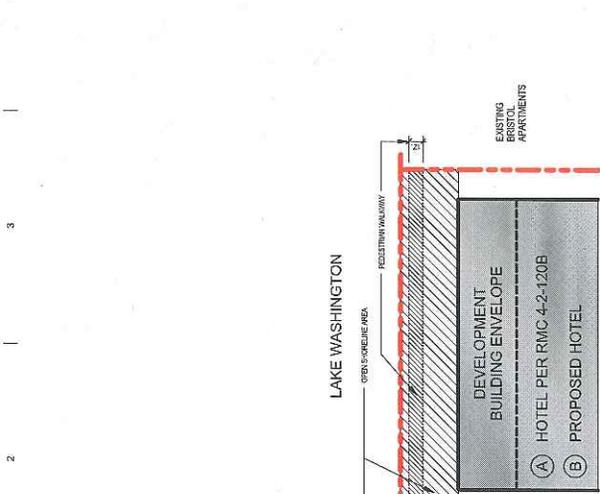
HOTEL PER RMC 4-2-120B UPPER STORY SETBACK

UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
JUNE 21 - NOON - SUMMER SOLSTICE



HOTEL PER RMC 4-2-120B UPPER STORY SETBACK

UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL - SOLAR ACCESS ANALYSIS  
JUNE 21 - 8AM - SUMMER SOLSTICE



HOTEL PER RMC 4-2-120B UPPER STORY SETBACK

UPPER STORY SETBACK, SHORELINE PUBLIC TRAIL SOLAR ACCESS ANALYSIS  
JUNE 21 - NOON - SUMMER SOLSTICE

**SOLAR ACCESS STUDY - OBJECTIVE**  
THIS SOLAR ACCESS STUDY DEMONSTRATES AND COMPARES SOLAR ACCESS CONDITIONS CREATED BY DEVELOPMENT (A) AND DEVELOPMENT (B) OVER THE OPEN SHORELINE AND PUBLIC ACCESS PORTION OF THE SHOWN.

SOLAR ACCESS STUDY - DETAIL KEY TABLE		DEVELOPMENT ENVELOPE TO COMPARE	
DAY & TIME OF SOLAR ACCESS		A - HOTEL PER RMC 4-2-120B	B - PROPOSED
WINTER SOLSTICE	NOON	(A)	(B)
DECEMBER 2	3:00PM	(A)	(B)
SPRING FALL EQUINOX	NOON	(A)	(B)
MARCH 20 & SEPT 23	3:00PM	(A)	(B)
SUMMER SOLSTICE	NOON	(A)	(B)
JUNE 21	2:30PM	(A)	(B)

(A) DEVELOPMENT WITH MAXIMUM ALLOWABLE BUILDING ENVELOPE, UPPER STORY ESTIMATED PER RMC 4-2-120B  
(B) DEVELOPMENT WITH PROPOSED, ALTERNATIVE BUILDING ENVELOPE, UPPER STORY SETBACK

**LEGEND**  
 AREA WITH SOLAR ACCESS  
 AREA IN SHADE (NO SOLAR ACCESS)

